

Urban Design Report

Official Plan and Zoning By-law Amendments Re: 340-376R Dufferin & 2 Melbourne Ave.
Hullmark Sunlife (376 Dufferin) LP – 21 July 2022



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Date: 21 July 2022

Hullmark Sunlife (376 Dufferin) LP
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Attn: Charles Arbez, Development Manager

**RE: Urban Design Report:
340-376R Dufferin & 2 Melbourne Ave., Toronto, ON**

Please find below my Urban Design Report on the proposed development at the municipal address of 340-376R Dufferin & 2 Melbourne Ave., Toronto, ON. This report accompanies the submission of an application for Official Plan and Zoning By-law amendments and Site Plan Approval.

The architects for the proposed development are Sweeny & Co Architects Inc. The documents on which this report is based are dated June 2022. The submission includes a Shadow Study (prepared by Sweeny & Co) and a Block Context Plan (prepared by Bousfields) that meet the City's Terms of Reference for such documents.

This report has been prepared in conjunction with the Planning Rationale Report prepared by Bousfields and should be read together with that report. In order to avoid duplication, this report will rely upon the analysis of the larger regulatory framework and general descriptions of the physical context that are contained in the Bousfields report.

The focus of this report is to set out the urban design approach of the proposed development and to relate that approach to the relevant City of Toronto Official Plan policies, Zoning By-law standards and Design Guideline documents.

1. Introduction

This urban design report concludes that the proposed development will create a unique and appropriate addition to its evolving South Parkdale context.

In my opinion, the subject property is an appropriate location for a combination of tall and mid-rise buildings. At 21, 25 storeys, 11, 6 and 4 storeys, the tall and mid-rise building elements of the proposed complex of buildings are in keeping with range of building heights found in this area.

The development context, which includes Dufferin Street to the east, Queen Street West to the north, and Melbourne Avenue to the south comprises a broad range of building types including a number of different mixed use and apartment building forms, from different periods and has been the subject of other significant planning applications.

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The proposed development represents an important and sensitive intensification of an existing cluster of industrial buildings that currently accommodate a mix of uses arrayed along Dufferin Street and around a courtyard, optimizing development potential of the eastern part of its urban block.

The design of the proposed building will fit appropriately into its context. Its built form represents a compatible addition to the urban district within which it is located.



Figure 1 Perspective View from Dufferin Street Looking South West (Sweeny &Co)

The proposed development will support the achievement of numerous policy directions promoting intensification within built-up urban areas, particularly because of its location in a part of the city that is well served by municipal infrastructure, including public transit.

From an urban design perspective, the proposal will create a high-quality architecturally distinctive addition to the existing neighbourhood, and will complement the area’s existing built form context. The proposal establishes appropriate relationships with the public realm, and has appropriate height, density, massing, scale and built-form, with adequate setbacks and step-backs.

The design and massing of the proposed new building, which includes the retention of several elements of the existing industrial buildings on the subject property, will complement the architectural expression of other nearby buildings which include a range of mixed use and residential building types.

The existing industrial buildings on the subject property are not listed on the City of Toronto’s Heritage Register nor are they designated under the Ontario Heritage Act. The subject property is adjacent to: 1-7 Melbourne Place, a pair of 1889 Georgian terraces that are listed on the City’s Heritage Register; and the southern boundary of the Parkdale Main Street Heritage Conservation District which is under study at the time of the writing of this report.

In my opinion, the proposed development meets the intent of the built form and massing policies and guidelines of:

- the Official Plan; and
- the City’s Tall Building Guidelines.



Figure 2 Context Aerial View from South West (Sweeny & Co)

2. The Subject Property

The subject property occupies corner site on the south-east of the urban block bounded on the north by Queen Street West, on the west by Gwynne Avenue, on the south Melbourne Avenue and on the east by Dufferin Street. The subject property is bounded on the north by Milky Way (a public laneway) and on the west by an abandoned laneway behind the residential properties that face Gwynne Avenue. On its narrower, southern part the subject property abuts the residential properties on Melbourne Place and those that face Melbourne Avenue. It is currently occupied by a mix of 1-3-storey mixed-use masonry clad commercial buildings which are organized around a surface parking area which occupies the balance of the property. Access to the surface parking area is provided via a driveway from Dufferin Street, located between the buildings at 342 Dufferin Street to the south and 358-360 Dufferin Street to the north.

The subject property has frontages of 129 metres on Dufferin Street and 39.3 metres on Melbourne Avenue and has an area of 7,444 square metres. The subject property is irregular in shape; the north end is approximately 72 metres deep while the south end has a depth of approximately 40 metres.

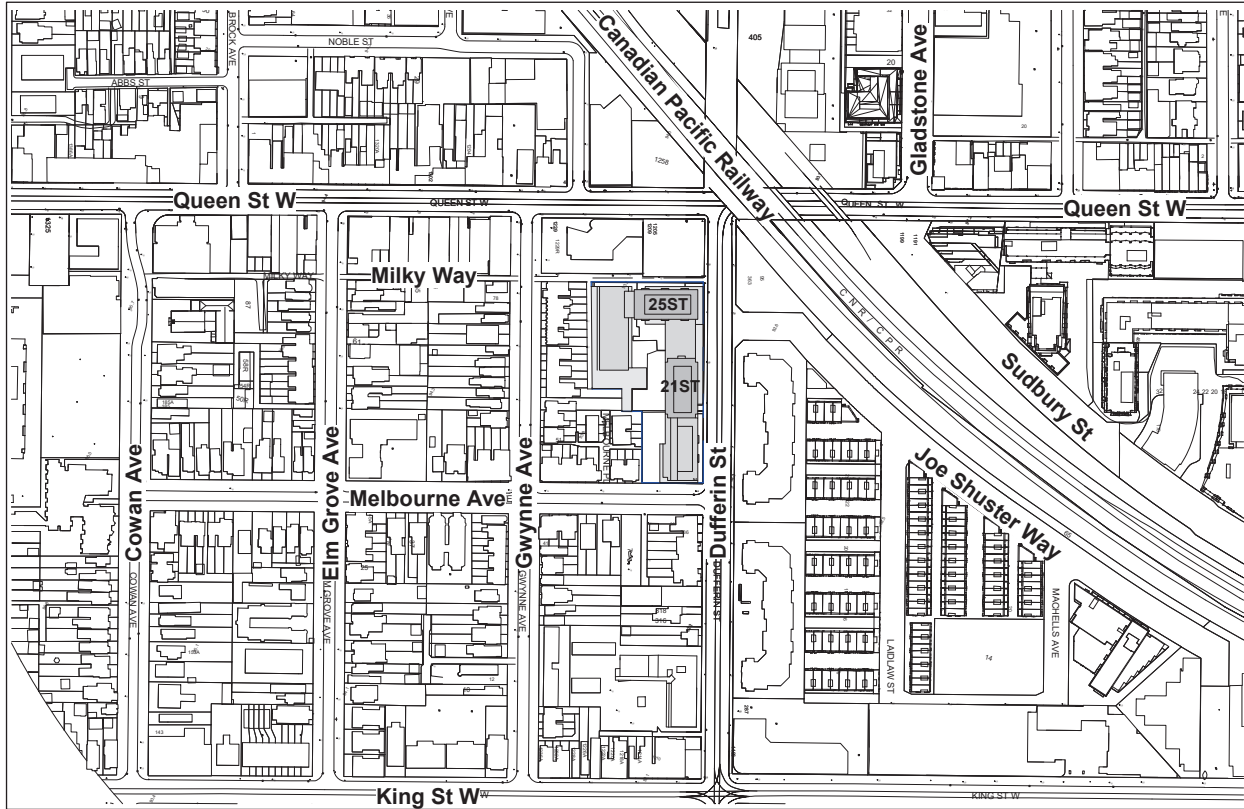


Figure 3 Context Plan (Sweeny & Co)

3. Built Form Context

South Parkdale, in the vicinity of the subject property, is characterized by older, low-rise commercial, retail and service uses along Queen Street, and residential neighbourhoods generally laid out according to an irregular grid to the north and south.



Figure 4 Aerial View from North West - Existing Conditions (Google)

In South Parkdale, this grid of streets is interrupted by the diagonal of the rail corridor that runs from the south east to the north west along the eastern edge of the neighbourhood. The rail corridor crosses the intersection of Queen Street West and the recently realigned Dufferin Street just to the east of the subject property. Historically, heavy industrial uses such as manufacturing and logistics operations occupied the properties in the vicinity of the rail corridor.

Existing buildings in the vicinity of the subject property to the north and east are remnants of this historical industrial activity. Many of these buildings have been converted to residential and live work occupancies as well as lighter employment uses such as offices and media production studios.

Abutting the north side of the subject property to the west is a private laneway. The laneway is approximately 3.0 metres wide, approximately 69.0 metres long and runs along the rear of the subject property, terminating generally at the mid-way point of the property. The laneway also runs parallel to the rear yards of the residential lots fronting Gwynne Avenue and, in this regard, provides a physical separation between the subject property and the neighbouring dwellings residential dwellings located on the east side of Gwynne Avenue (87-63 Gwynne Avenue). Currently, the laneway provides access to the western side of the subject property and to surface parking pads that serve the residential lots along Gwynne Avenue. The laneway would continue to provide access to the existing rear-lane parking pads and would be improved through the redevelopment.

The residential neighbourhood to the west consists of a mix of low-rise residential uses in the form of 2- and 3-storey detached and semi-detached dwellings, rowhouses and multi-unit residential apartment buildings. The neighbourhood consists of largely low-rise residential uses with the exception of a few mid-rise apartment buildings on Elm Grove Avenue, Cowan Avenue and Dunn Avenue and a high-rise apartment building (20 storeys) at 245 Dunn Avenue.

The neighbourhood also includes a range of institutional and community uses, such as the Elm Grove Living Centre nursing home (35 Elm Grove Avenue); the St. John's Polish National Catholic Church (186 Cowan Avenue); the Epiphany and St. Mark Anglican Church and Community Garden (201 Cowan Avenue); the Holy Family Catholic Academy (141 Close Avenue); a community park with a splash pad (Masaryk Park) and the Dunn Avenue Parkette. Further west is Parkdale Collegiate Institute (209 Jameson Avenue) and a number of apartment buildings ranging in height between 5 and 12 storeys fronting both sides of Jameson Avenue.

To the west of the subject property and along the south side of Queen Street West is the Toronto Public Library, Arts & Culture Centre, a Toronto Parking Authority parking lot and the Masaryk-Cowan Community Recreation Centre. The properties are City-owned and form the Parkdale Community Hub.

To the northwest of the subject property are a mix of low-rise residential dwellings and the approved 7-storey (24.6 metre) residential development at 57 Brock Avenue and the West Lodge Apartment Building complex, comprised of two 16-storey apartment buildings.

Abutting the subject property to the north is Milky Way, a 3.7-metre-wide City-owned public right-of-way that functions as a laneway. Milky Way traverses east-west between Dufferin Street to the east and Cowan Avenue to the west and provides access to the rear of the properties along the south side of Queen Street West.

On the north side of Milky Way, at the southwest corner of Queen Street West and Dufferin Street, is a 7-storey mid-rise building (1205 Queen Street West) with at-grade retail uses. Abutting the mid-rise building to the west is the Parkdale Queen West Community Health Centre.

On the north side of Queen Street West is a 2-storey commercial building, the Parkdale Amphitheatre, and the Canadian National and Canadian Pacific rail corridor. Just west of the rail corridor and on the south side of Noble Street, the property at 6 Noble Street is planned for an 8-storey (28-metre) mid-rise mixed-use building; the development was approved by the LPAT (now OLT) on January 28, 2019. Further north, on the north side of the rail corridor and on the west side of Dufferin Street are three recently-constructed mixed use buildings (13, 9 and 10-storeys) at 390-444 Dufferin Street. Further north, the area is characterized by low-rise residential uses.

On the north side of the rail corridor to the northeast of the subject property, the area has recently undergone significant change with the introduction of several mid-to-high-rise mixed-use developments. Within the block bounded by Queen Street West, Gladstone Avenue, Peel Avenue and Dufferin Street are five mixed-use buildings that range in heights between 5 and 20 storeys. At the northwest corner of the block is Pessoa Park (405 Dufferin Street), a new public park with a splash pad, which opened to the public in 2020.

At the northeast corner of Queen Street West and Gladstone Avenue is the Gladstone Hotel (1214 Queen Street West), which is designated as a Part IV heritage building in accordance with the Heritage Act. Further west are a handful of 2- and 3-storey mixed-use main street commercial buildings fronting Queen Street West. Behind the Gladstone Hotel is a large grocery store (Fresh Co) located at 22 Northcote Avenue and low-rise residential uses beyond.

To the east, at the southeast corner of Dufferin Street and Queen Street West, is a vacant triangular lot that is currently utilized as a surface parking lot. To the south of the parking lot, south of Joe Shuster Way, are two 8-storey slab-style apartment buildings (295 and 345 Dufferin Street). Further east is a townhouse development comprised of 3- and 4-storey townhouse units and Rita Cox Park. East of the railway tracks and south of Queen Street West, are recent high-rise mixed-use developments with heights that range between 6 and 21 storeys.

To the southeast of the subject property, on the south side of Sudbury Street at Lisgar Street, is the planned King-Liberty SmartTrack Station that will utilize the existing GO rail corridor. The station will provide service between Union and Bloor Stations. The station will be accessible via two pedestrian bridges between Sudbury Street and Joe Shuster Way, approximately 450 metres (an approximate 5-minute walk) from the subject property.

To the immediate south of the subject property is Melbourne Avenue. South of Melbourne Avenue and along the west side of Dufferin Street, are 2-storey rowhouse dwellings (324-338 Dufferin Street). Abutting the rowhouses to the west is Melbourne Avenue Parkette. Further south along Dufferin Street are additional low-rise residential dwellings, a surface parking lot and a 7-storey residential apartment building (300 Dufferin Street).

Continuing south, two mixed-use buildings (14-storeys and 17-storeys) are planned at the southwest and northeast corners of King Street West and Dufferin Street at 1221 King Street West and 1182 King Street West respectively. Further south is the King-Dufferin Parkette, a neighbourhood park that features a splash pad and a children's playground. South of the King-Dufferin Parkette is a 14-storey slab-apartment building known as The Lord Dufferin (200 Dufferin Street). The remainder of uses south of King Street West are primarily characterized by mid-to-high rise apartment buildings and low-rise residential uses.

4. Road Network

The subject property fronts onto Dufferin Street, an important north-south corridor that spans the length of the City. North of Bloor Street, Dufferin Street is classified in the City of Toronto's Road Classification system as an arterial road, however, the portion of Dufferin Street fronting on the subject property is classified as Minor Arterial road with an existing and planned right-of-way width of 20 metres, as shown on Map 3, Official Plan. Dufferin Street includes four lanes of vehicular traffic, two lanes in each direction, with on-street parking available on the west side and pedestrian sidewalks on both sides of the street. The central lanes of traffic share the right-of-way with streetcars.

The subject property abuts Milky Way to the north; a 5.0-metre-wide City-owned public right-of-way that traverses east-west between Dufferin Street to the east and Cowan Avenue to the west. Milky Way is proposed to be widened to contribute to an ultimate laneway width of 6.0 metres, which will facilitate traffic movement and servicing for the proposed building.

Melbourne Avenue, which fronts the subject property to the south, is two-way, two-lane Local Road with an existing right-of-way of 20 metres.

Queen Street West, located approximately 40 metres to the north of the subject property, is another important corridor that spans east-west across the length of the City. The City of Toronto's Road Classification System identifies Queen Street West as a Major Arterial road with an existing and planned right-of-way width of 20 metres. Queen Street West includes four lanes of vehicular traffic, two lanes in each direction, with on-street parking and pedestrian sidewalks on both sides of the street. Similar to Dufferin Street, the central lanes of traffic share the right-of-way with streetcars.

5. Transit Network

The subject property has excellent access to public transit services, including planned higher-order transit.

Dufferin Street is identified as a Transit Priority Segment within the City's Enhanced Surface Transit Network, as shown on Map 5 of the Official Plan. The subject property is directly serviced by the 29 Dufferin bus, which operates between Wilson Station on Line 1 (Yonge-University) and Exhibition Place, generally in a north-south direction. It also serves Dufferin Station on Line 2 (Bloor-Danforth). This route is part of the 10-minute network and operates 10 minutes or better, all day, every day. The closest southbound bus stop is located at the southeast corner of the subject property (the northwest corner of Dufferin Street and Melbourne Avenue) and the closest northbound stop is located on the east side of the street, approximately 65 metres south of the subject property (a 1-2 minute walk).

In addition, the Dufferin corridor, along with other major routes across the City, has been flagged in the TTC's 5-Year Service Plan & 10-Year Outlook as an Enhanced Priority Corridor where the TTC is planning to implement Bus Rapid Transit services.

Queen Street West, located approximately 40 metres to the north of the subject property, is also identified as a Transit Priority Segment within the City's Enhanced Surface Transit Network, as shown on Map 5 of the Official Plan. Along Queen Street West, the subject property is served by eastbound and westbound stops on the 501 Queen streetcar route located on the north and south sides of the intersection of Queen Street West and Dufferin Avenue. The 501 Queen streetcar route operates between Neville Park Loop and Long Branch

Loop, generally in an east-west direction. It serves Queen and Osgoode Stations on Line 1 Yonge-University. Both Queen and Osgoode Stations are accessible subway stations.

It is also within walking distance of several transit routes including:

- 402 Parkdale Community bus – serves the Parkdale community with connections to Dufferin Subway Station on Line 2.
- 47 Lansdowne bus – runs generally in a north-south direction from Queen Street West to St. Clair West.
- 501 Queen streetcar route – operates between the Neville Park Loop and the Long Branch Loop, generally in an east-west direction. It serves Queen and Osgoode Stations on Line 1 Yonge-University. Both Queen and Osgoode Stations are accessible subway station.
- 504 King streetcar route – operates between Dundas West Station and Broadview Station on Line 2 Bloor-Danforth, generally in an east-west direction. It also serves the St Andrew and King stations on Line 1 Yonge-University. Dundas West, St. Andrew, and Broadview stations are accessible. The 504 King B eastbound and westbound traverse north and south at the Dufferin Gate Loop just north of Exhibition Place

Future Transit:

The City has identified six locations for future SmartTrack transit stations, including one at King-Liberty on the Kitchener GO line that will provide service between Union and Bloor Stations. The King-Liberty GO Station will be located on King Street West, near Hanna Avenue and Atlantic Avenue, with station access from King Street West, Joe Shuster Way and Sudbury Street. The planned entrance closest to the subject property is located on Joe Shuster Way, approximately 85 metres north of King Street West. The station entrance is approximately 450 metres southwest of the subject property resulting in an approximate 5-minute walk from the subject property.

In addition, the subject property is located approximately 1.2 kilometres north of the existing Exhibition GO Station, which currently accommodates GO and VIA Rail services. As part of the Ontario Government Transit-Oriented Communities program, Exhibition Station is planned to operate as a transit hub, connecting to local buses, GO Transit and subway service along the Ontario Line. Based on preliminary information released by Metrolinx, the station will include an above-ground shared concourse between the Ontario Line and the existing Lakeshore West GO Transit services to provide easy transfer access to transit users.

In April 2019, the Provincial Government released its transit plans for the GTHA, which included a downtown line called the Ontario Line. The most southerly station on the proposed Ontario Line is the Exhibition (Ontario Place), approximately 1.2 kilometres (or a 20-minute walk) from the subject property.

6. Cycling Network

The subject property is also served by existing and planned cycling infrastructure.

Existing cycling infrastructure can be found on Brock Avenue and a small section of Dufferin Street at Queen Street West underneath the rail bridge and extending north to Peel Avenue. Both bike lanes travel in a north-south direction.

Construction of extension of the West Toronto Railpath, from Sterling Avenue, along the Kitchener GO rail corridor to just south of Queen Street West at Abell Street, located to the northeast of the subject property is planned to begin in late 2022.

7. The Proposed Development

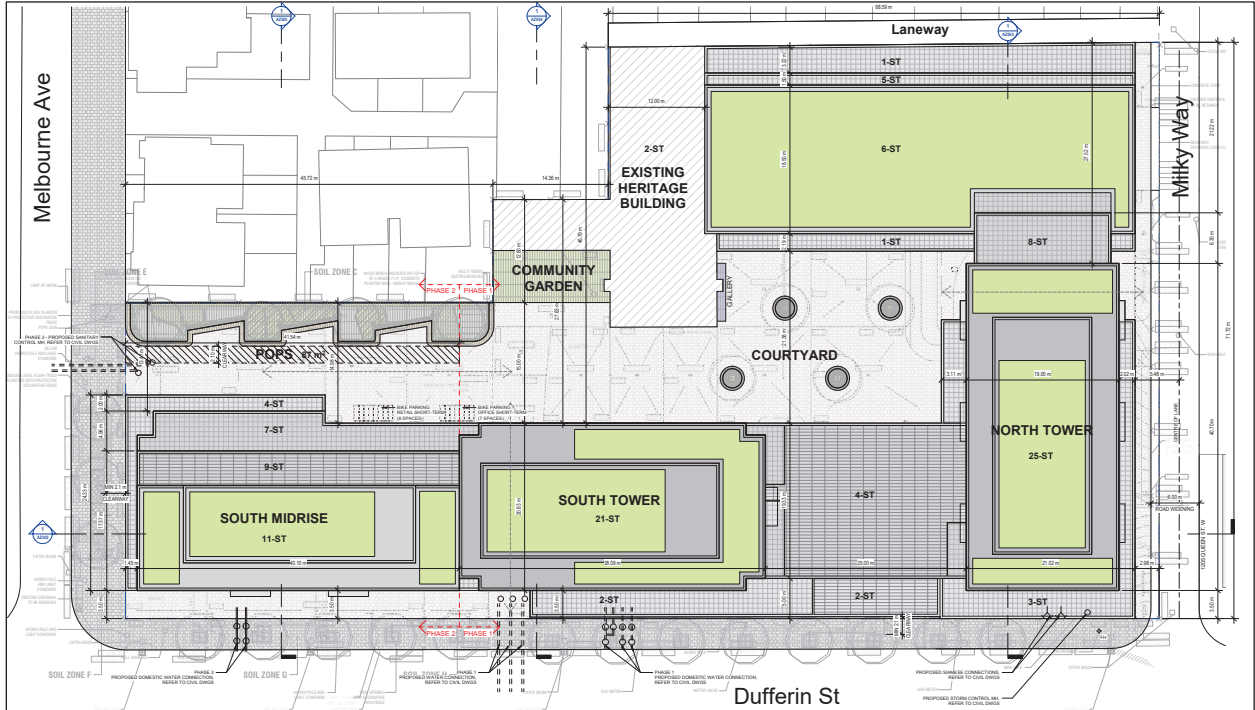


Figure 5 Site Plan (Sweeny&Co)

The proposed development consists of two tall building elements above a series of linked base buildings surrounding a mid-block, at-grade, courtyard.

Development will be phased. The first phase includes the two tall buildings and the majority of the proposed base buildings on the northern part of the property facing Dufferin Street and Milky Way. The second phase includes the development of the southern portion of the property facing Dufferin Street and Melbourne Avenue in the form of a mid-rise building.

Descriptions and opinions which follow in this report refer to the complete development unless specifically indicating reference to the first phase only.

The proposed development includes the retention and full conservation of an existing mid-block 2-storey former industrial building (350 Dufferin Street) and the retention and incorporation of the facades of the existing 2-storey former industrial buildings that line a portion of the Dufferin Street and western laneway frontages (358 and 360 Dufferin Street respectively).

The mid-block courtyard space includes a private space which is similar in configuration to the surface parking area that currently exists on the subject property. The new courtyard space is proposed to be publicly accessible. Initially there will be two access points that pass through the proposed new buildings, one at the southern end of the first phase building and one on the north from Milky Way. Once the second phase is

complete, the courtyard space will be extended south to Melbourne Avenue. This extension will be, in part, in the form of a privately owned public space (“POPS”) which will be incorporated in a third pedestrian access to the courtyard.

The Courtyard and POPS are proposed to be paved masonry which extends the industrial aesthetic of the existing building complex and the proposed new buildings.



Figure 6 from Milky Way Looking South (Sweeny&Co).



Figure 7 View of Courtyard Looking South (Sweeny&Co)

The proposal includes a 3.0 metre setback from Milky Way, in addition to the proposed widening, to accommodate a pedestrian clearway along its south side. There is no setback proposed relative to the private laneway along the west edge of the subject property, however its pavement will be improved.

The proposed building includes a 2-level below grade parking structure. Bicycle storage, with elevator access is located on the P1, ground and second floor levels. Car parking will take place on the P1-P2 levels. Access to the car parking spaces on these levels will be from a ramp located in the north west part of the subject property with access from Milky Way, a public laneway. Garbage, loading and moving facilities are also located facing Milky Way, in the north central part of the subject property.

7.1. Ground and Second Floors and Courtyard/POPS

The ground floor of the proposed base buildings includes: “flex-commercial” uses and three residential lobbies lining Dufferin Street and Melbourne Avenue; “flex-studio” spaces and an associated lobby in the north west facing the courtyard and Milky Way; and a loading facility with 1 Type G/B garbage/loading/moving space with access from Milky Way. Residential, “flex-retail” and “flex-studio” garbage rooms are located on the P1 level with elevator access to the garbage/loading/moving space on Milky Way.

The “flex-commercial” spaces located along Dufferin Street have the potential for frontage on both the street and the courtyard. Access to those located in the Phase 2 building on the southern part of the subject property have their street entrances from a recessed area at grade. Access to those located behind the retained façade, and the central of the three residential lobbies is through modified existing openings that provide accessible entrances. The “flex-commercial” space in the first phase facing the courtyard include a covered outdoor space created by a proposed two-storey high colonnade.



Figure 8 Courtyard Entrance from Dufferin Street (Sweeny&Co) Figure 9 Courtyard Looking North (Sweeny&Co)

The “flex-studio” spaces are located on the ground and second floors of the building in the north west part of the proposed development. There are “flex-studio” units on the ground floor that have direct access to the proposed courtyard space and others which face the existing upgraded private laneway along the west edge of the subject property.



Figure 10 Courtyard View Flex Studio (Sweeny&Co) Figure 11 Courtyard View Heritage Building (Sweeny&Co)

The retained heritage building also houses flex studio space. It is augmented by the construction of a new greenhouse space to the south and a publicly accessible gallery component is proposed for its north face. The proposed development is served by 4 separate elevator cores.

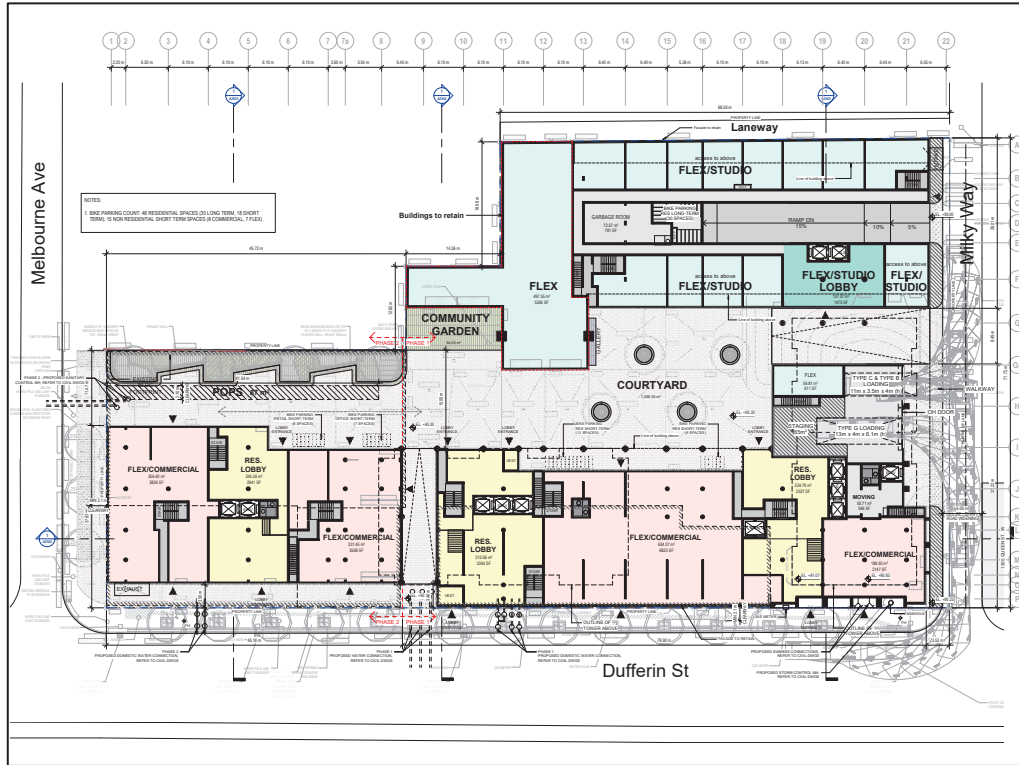


Figure 12 Ground Floor Plan (Sweeny & Co)

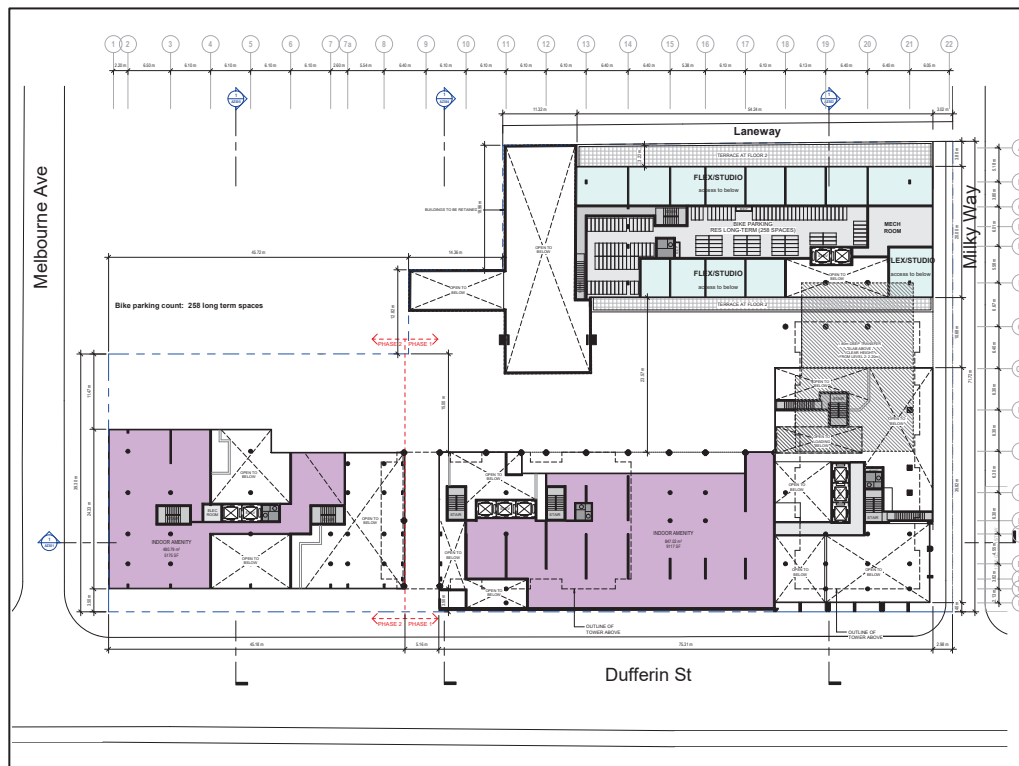


Figure 13 2nd Floor Plan (Sweeny & Co)

Indoor amenity spaces occupy the majority of the second-floor. “Flex studio” spaces are located on the north west second floor. These studio spaces have outdoor terraces facing either the courtyard or the private laneway to the west.

7.2. Mid Rise Building Elements

The proposed development includes two mid-rise building elements, a 6-storey element at the north west of the subject property, part of the north block, which accommodates the “flex-studio” spaces on the ground and second floors described above that is part of the first phase and an 11-storey element in the south west, part of the south block, that is part of the second phase.

Above the ground floor, the 6-storey building is stepped back between 3.2 and 3.8 metres from the private laneway on the west and 2.19 metres from the courtyard on the east. An additional 1.5 metre stepback is proposed above the 5th floor on the west. The 6-storey building engages the 8-storey base building of the north tower and the retained existing heritage building on the south. 2 storey residential units are included on the 3rd and 4th floors. The 5th and 6th floors include conventional 1-storey units. There are no residential balconies in the 6-storey building. There is, however, 1.5 metre deep terrace in a stepback along the west face of the 6th floor.

Above the 4th floor, the 11-storey building is stepped back 2.0 metres on the south west facing the POPS and 1.45 metres on the south facing Melbourne Avenue. A 5.0 metre step back is provided on the west facing the POPS above the 7th floor above which is an additional 4.31 metre stepback at the 10th floor to create a terrace which accommodates a roof top outdoor amenity space, which also includes some areas that are stepped back even further. The 11-storey building engages the south face of the base and tower elements of the South Tower but is designed as an independent building. The south face of the South Tower, above grade is the demising line between the two phases of the proposed development.

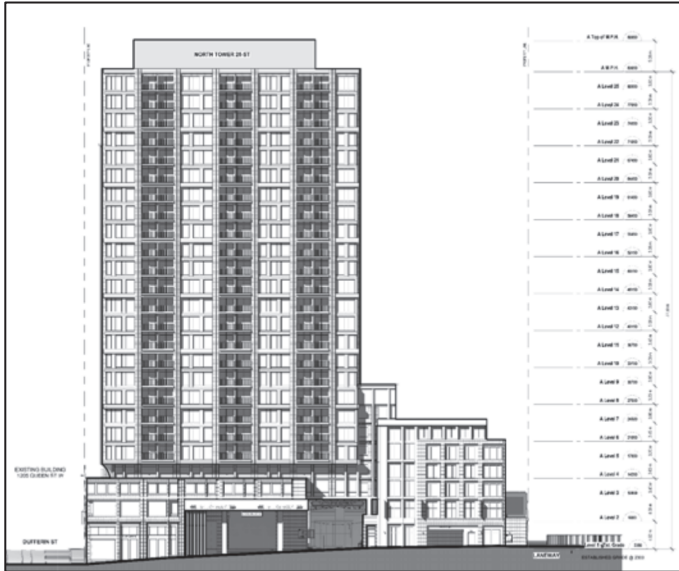


Figure 14 North Elevation (Sweeny&Co)

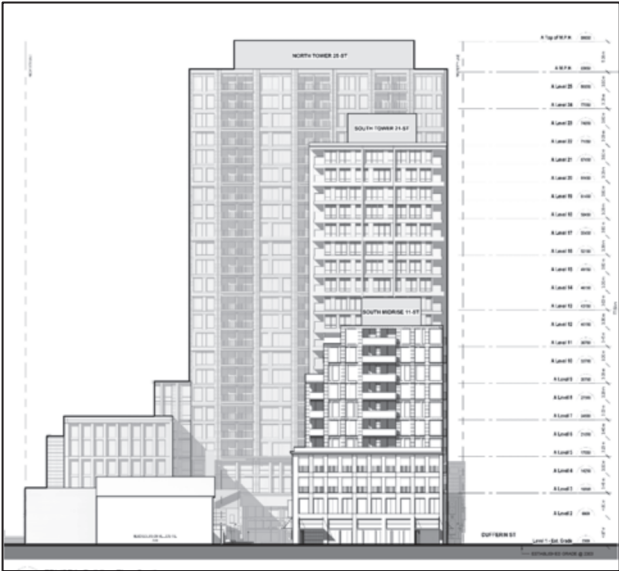


Figure 15 South Elevation (Sweeny&Co)

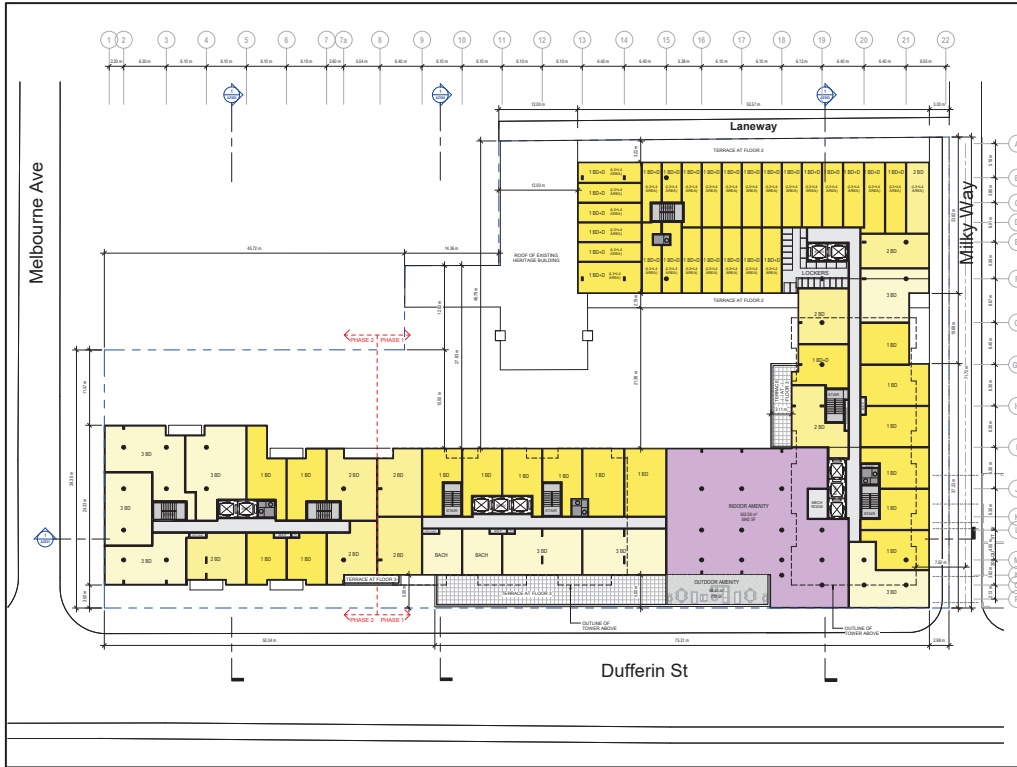


Figure 16 3rd Floor Plan (Sweeny & Co)

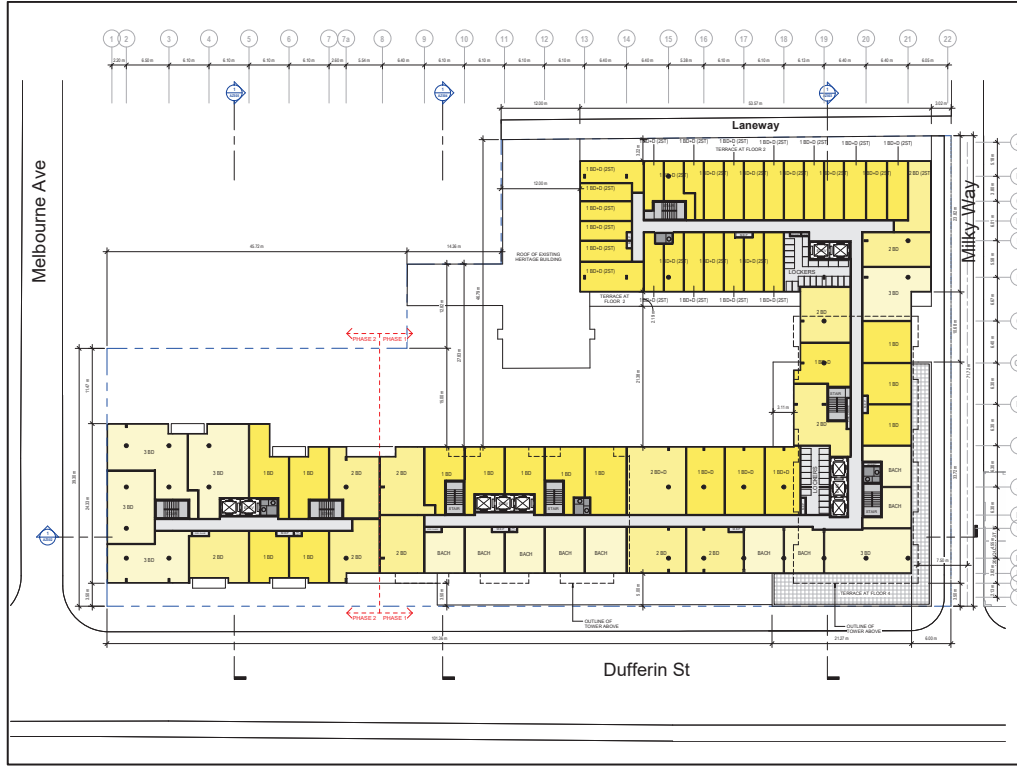


Figure 17 4th Floor Plan (Sweeny & Co)

7.3. Tall Building Base Elements

The third and fourth floors of the tall building base elements and the mid-rise buildings, containing residential units, wrap continuously around the courtyard space on the west north and east, with the south and south west edges open.

The fifth floor introduces a break in the enclosure of the courtyard in the form of a roof top outdoor amenity space located in the north east. At this level the proposed development consists of an “L” shaped northern block and a linear southern block, which continues in the sixth floor. The outdoor space has exposures to both Dufferin Street and the courtyard. Access to the roof top outdoor amenity space is from the residential corridors in the south block and through an indoor amenity space in the north block.

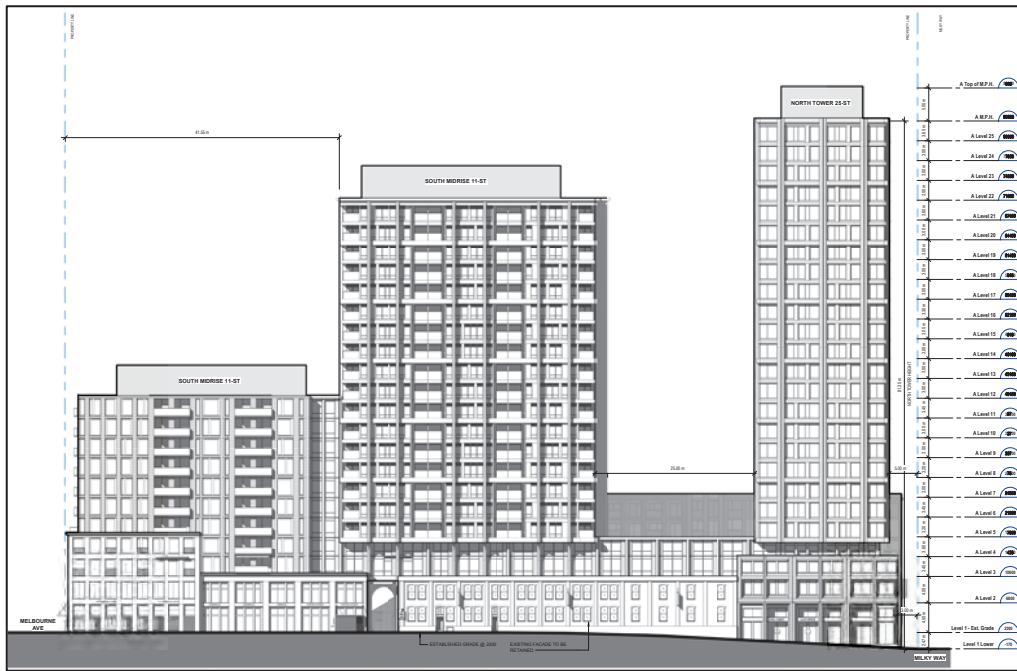


Figure 18 East Elevation (Sweeny&Co)

The base building of the North Tower is 3 storeys in height facing Dufferin Street, where it aligns with the retained existing building façade to the south, and along the east part of Milky Way. It accommodates residential units above a tall “flex commercial” ground floor, the service and the indoor amenity spaces and the entrance point connecting Milky Way and the courtyard described above.

The base building of the North Tower is clad in a highly articulated masonry frame with deeply inset windows. with

The base building of the South Tower is 4 storeys in height and includes 2 storeys setback by 5.0 metres above the retained existing masonry building facing Dufferin Street. It accommodates residential units above a tall “flex commercial” ground floor described above.

Above the 2nd floor, the base building of the South Tower is clad in a 2-storey light-coloured aluminum frame with recessed deeply inset aluminum glazing and spandrel panels which aligns with the 4th floor of the North tower to create a “reveal” along Dufferin Street above which both the North and South Towers project forward.

7.4. Tall Building Tower Elements

The proposed tall building elements start at the 4th floor on the north block and the 5th floor on the south block where the floor plates of the tall building components become visible.

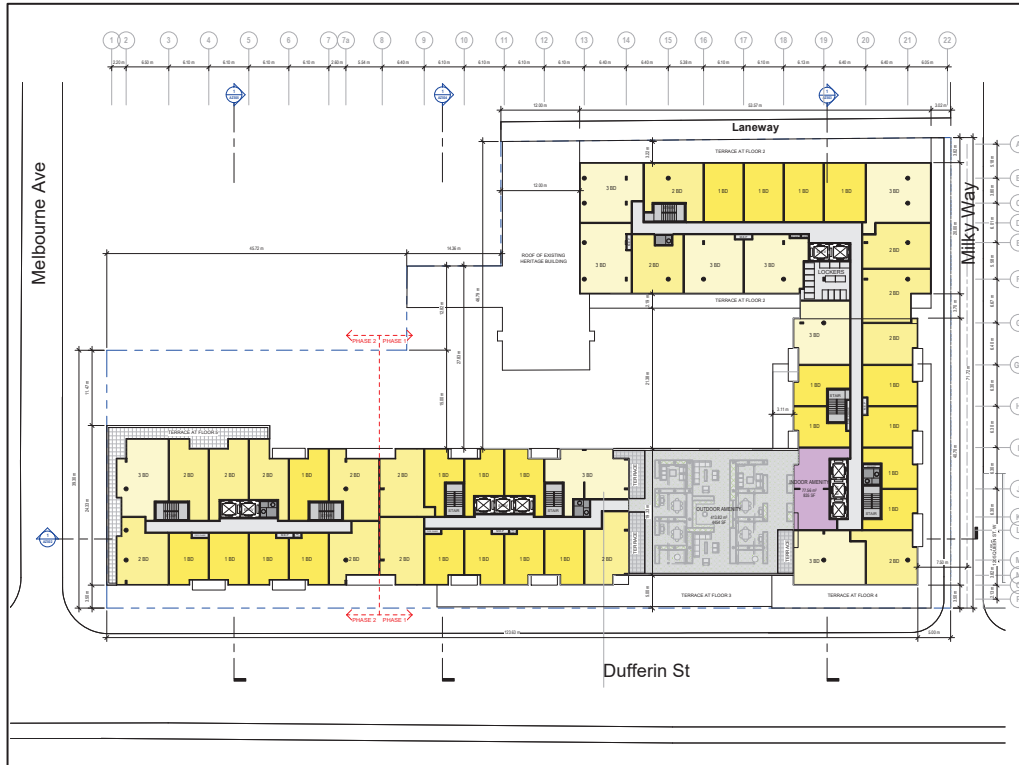


Figure 19 5th Floor Plan (Sweeny & Co)

The North Tower has a larger floor plate on the 6th - 8th floors above which it steps back to the east to form a rectangular tower floor plate of approximately 750 square metres. The North Tower has a total height of 25 storeys.

The South Tower is rectangular in form with a tower floor plate of approximately 750 square metres. The South Tower has a total height of 21 storeys.

Recessed or partially recessed balconies are proposed on both towers.

The proposed towers are separated from one another by 25.0 metres. The proposed towers will achieve other tower setbacks as follows:

North Tower

- Approximately 27.5 metres above the 8th floor to the west edge of the private laneway to the west;
- Approximately 32.5 metres to the rear lot lines of the *Neighbourhoods* designated properties facing Gwynne Avenue to the west;

South Tower

- Approximately 27.8 metres above the 11th floor the rear lot lines of the deepest of the *Neighbourhoods* designated properties facing the southern part of Gwynne Avenue to the west (#59 and #61);
- Approximately 46.79 metres to the west edge of the private laneway to the west;
- Approximately 51.79 metres to the rear lot lines of the *Neighbourhoods* designated properties facing the northern part of Gwynne Avenue to the west;
- Approximately 14.98 metres to the side lot lines of the *Neighbourhoods* designated property facing Melbourne Avenue to the west (#1); and
- Approximately 14.98 metres to the rear lot lines of the *Neighbourhoods* designated properties facing Melbourne Place to the west (#1, #3, #5, #7) which are listed in the City’s Heritage Register.

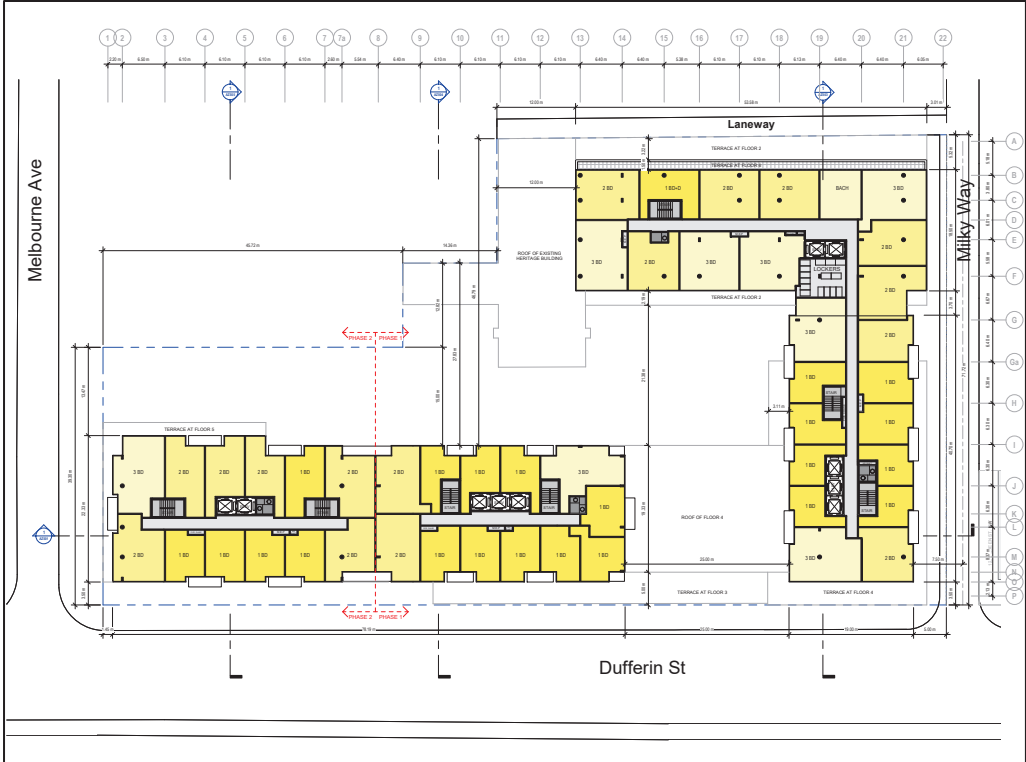


Figure 20 6th Floor Plan (Sweeny & Co)

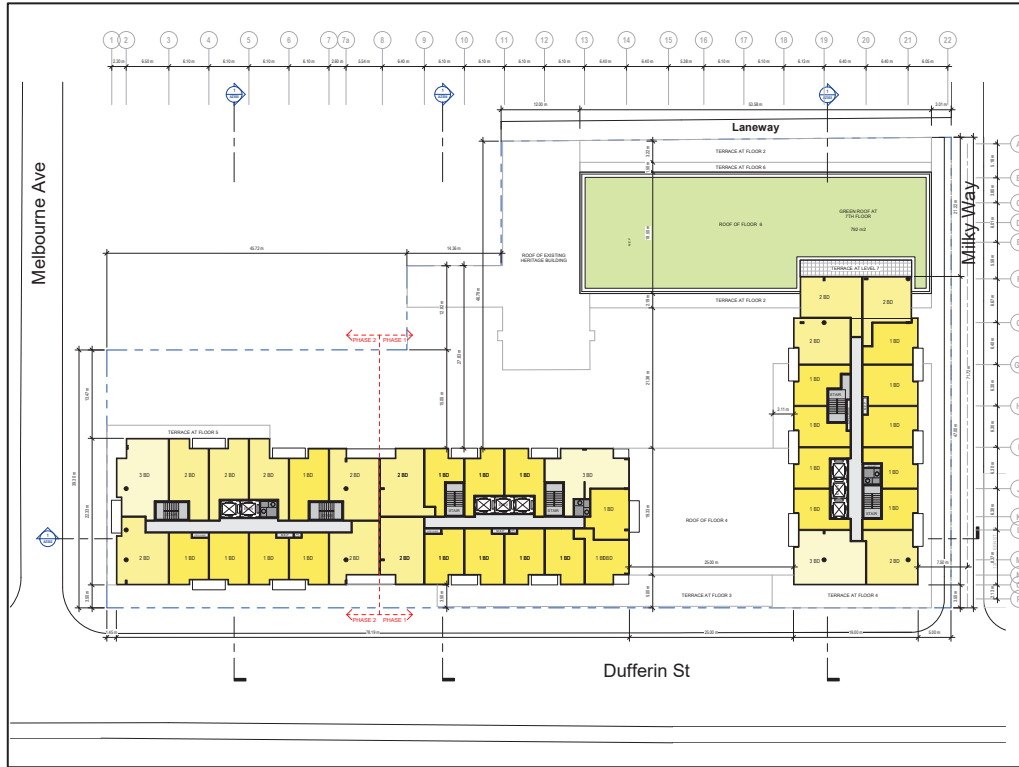


Figure 21 7th Floor Plan (Sweeny & Co)

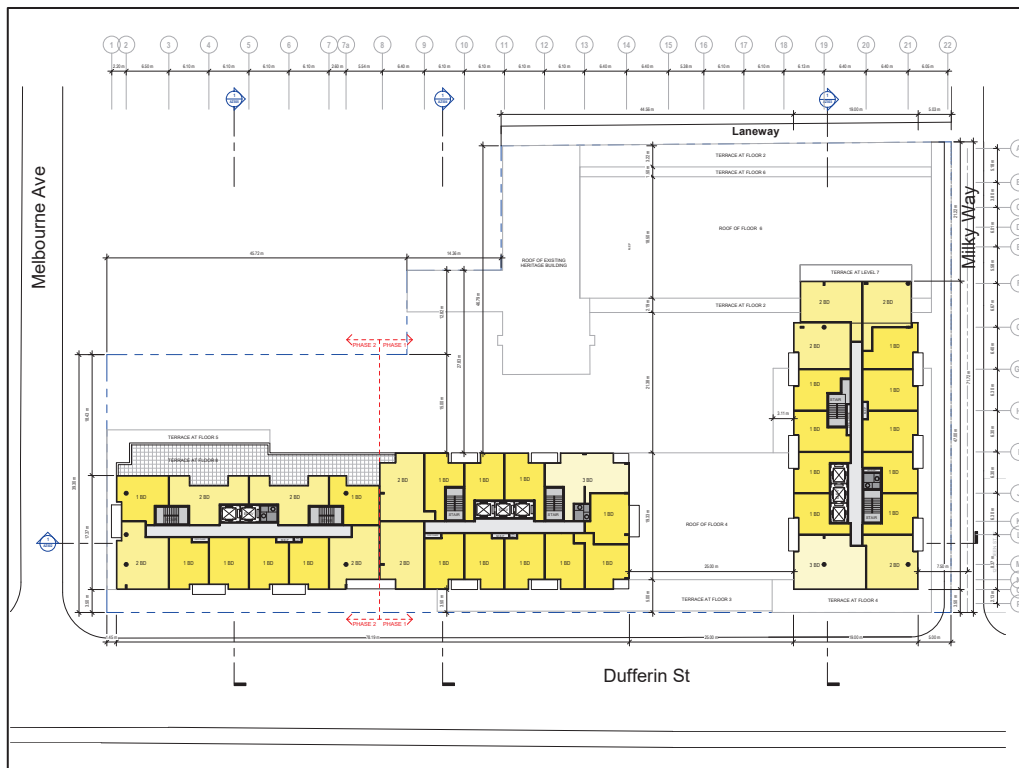


Figure 22 8th Floor Plan (Sweeny & Co)

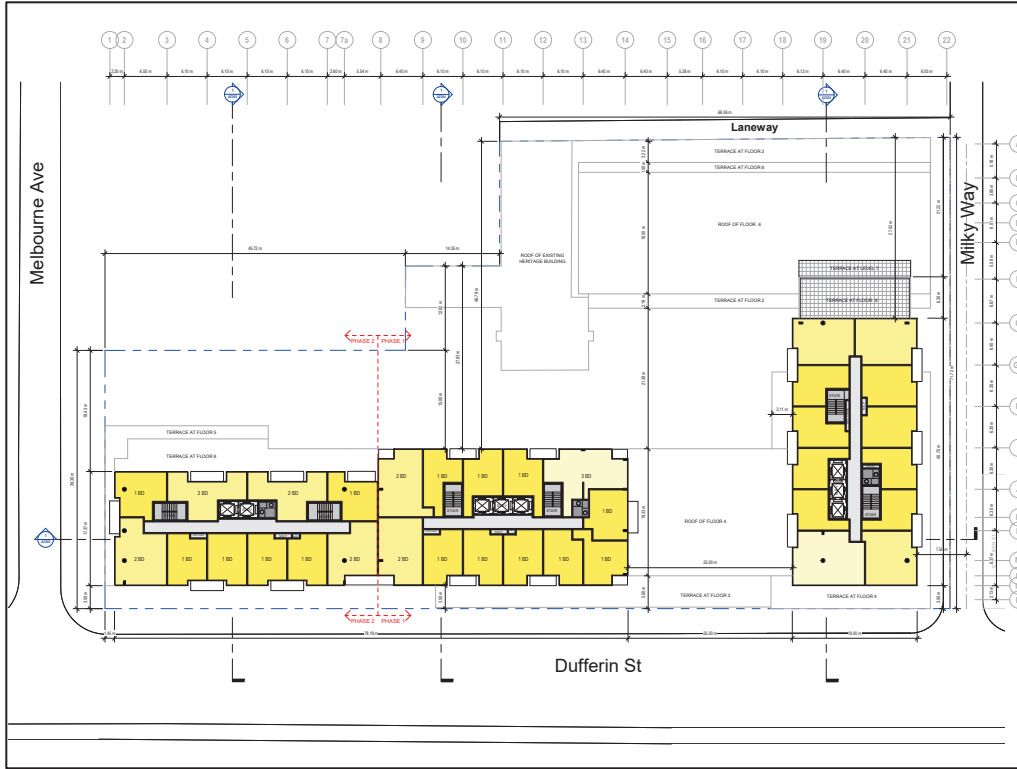


Figure 23 9th Floor Plan (Sweeny & Co)

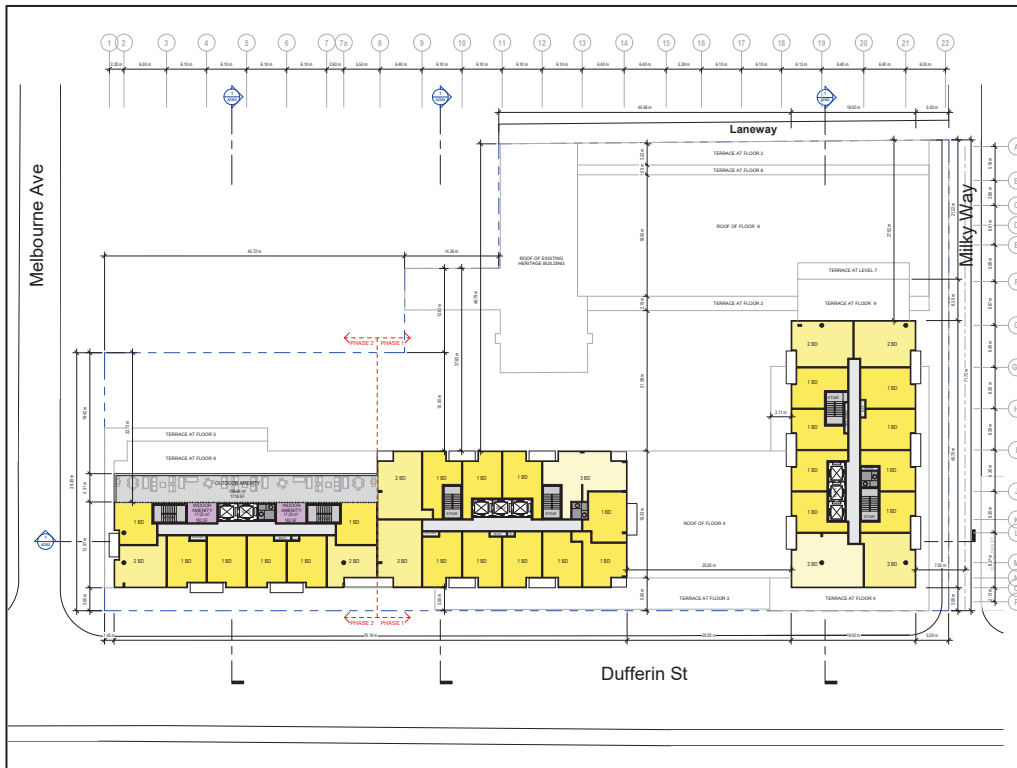


Figure 24 10th Floor Plan (Sweeny & Co)

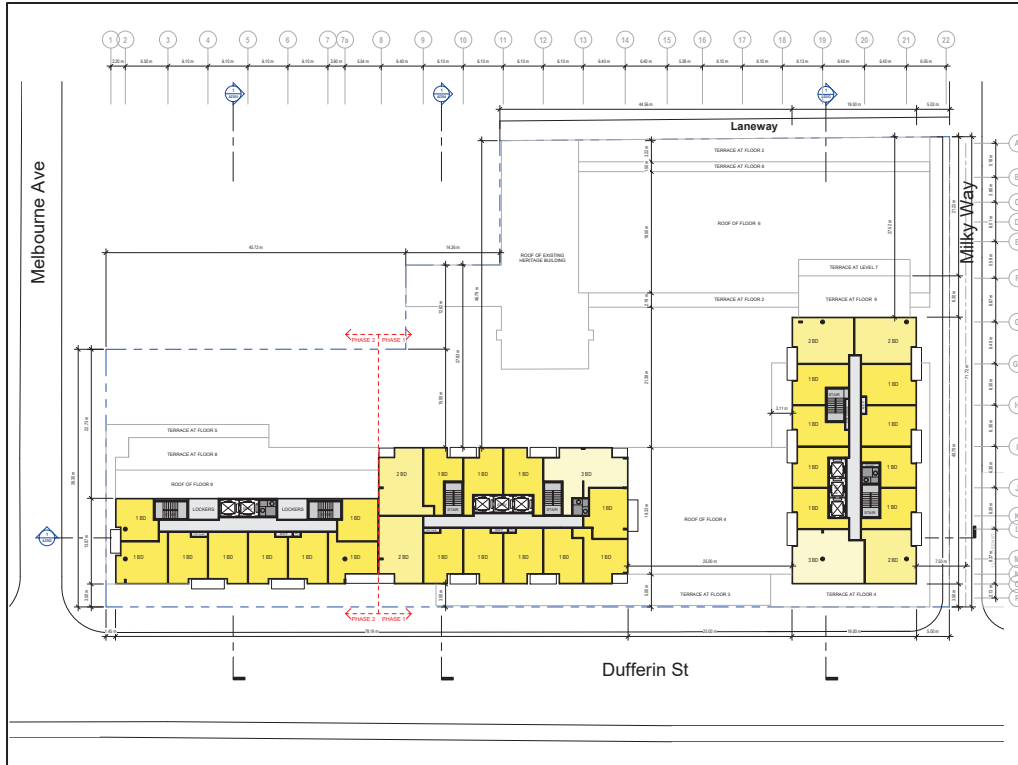


Figure 25 11th Floor Plan (Sweeny & Co)

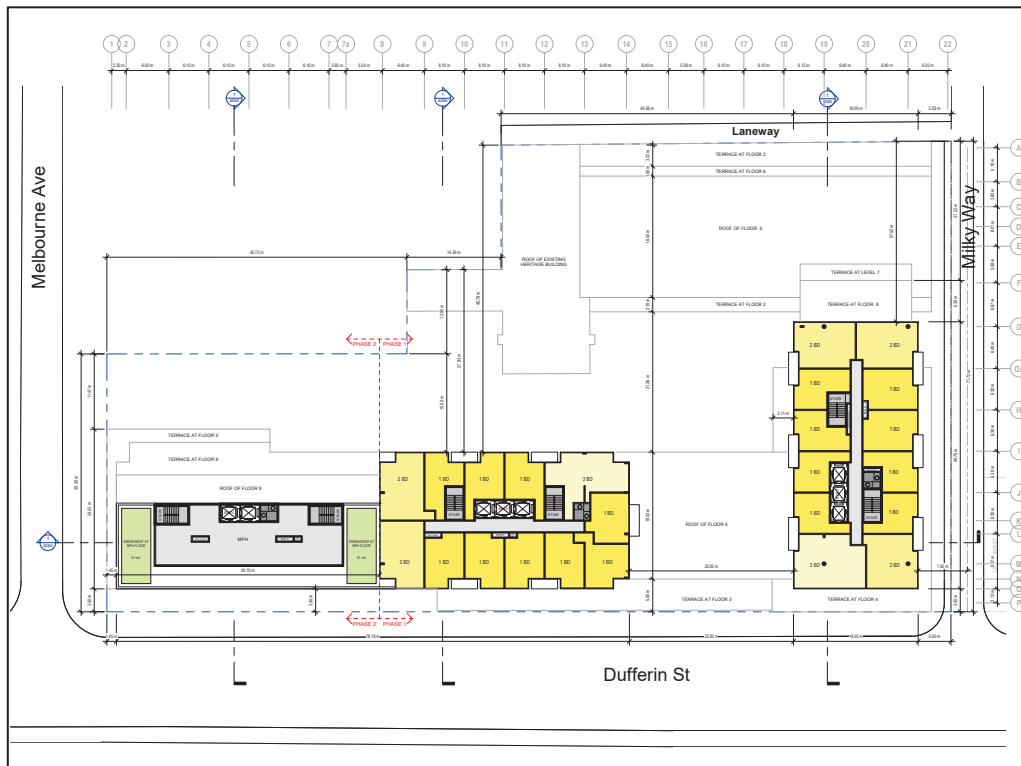


Figure 26 12th Floor Plan (Sweeny & Co)

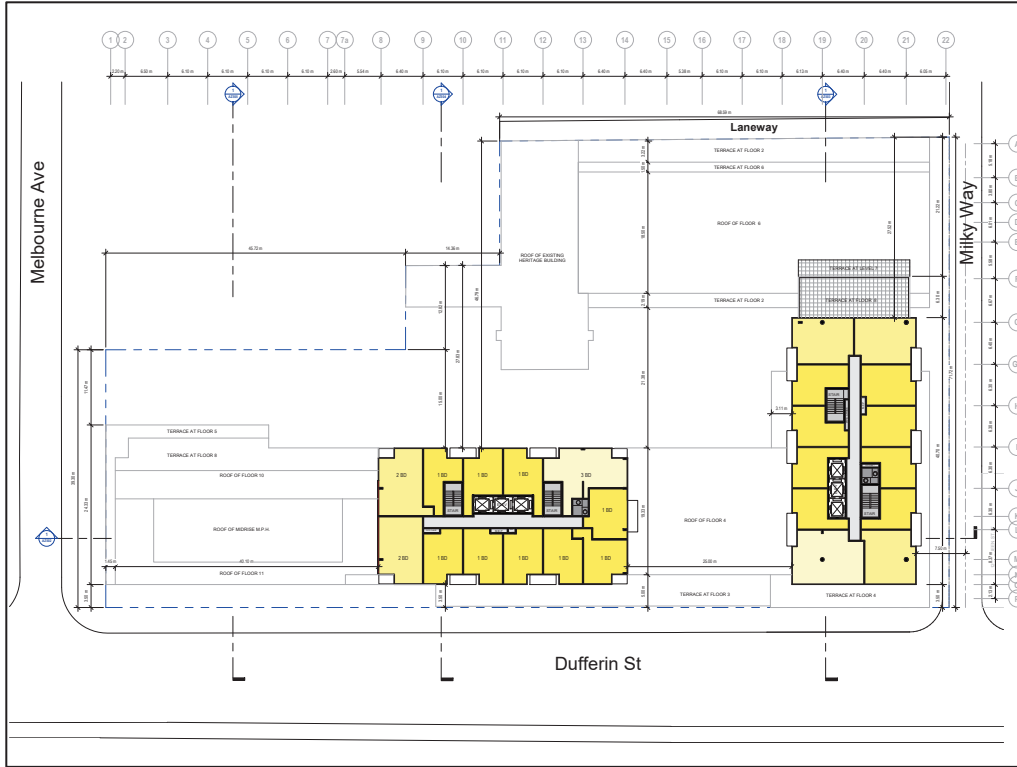


Figure 27 13th - 21st Floor Plan (Sweeny & Co)

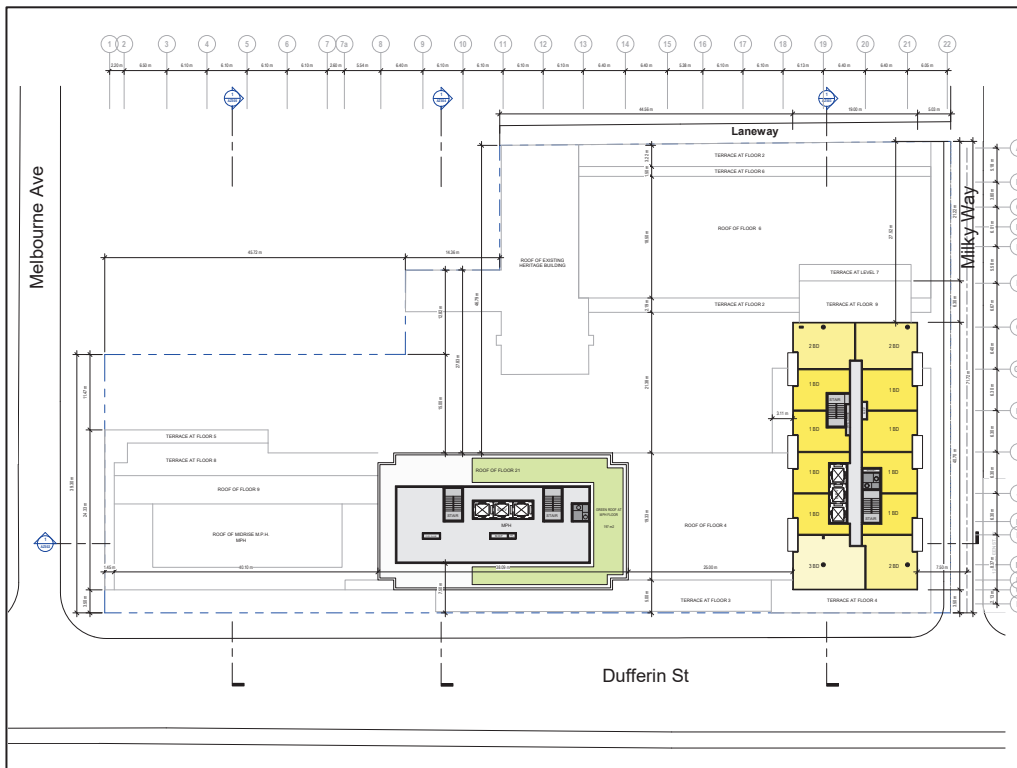


Figure 28 22nd Floor Plan (Sweeny & Co)

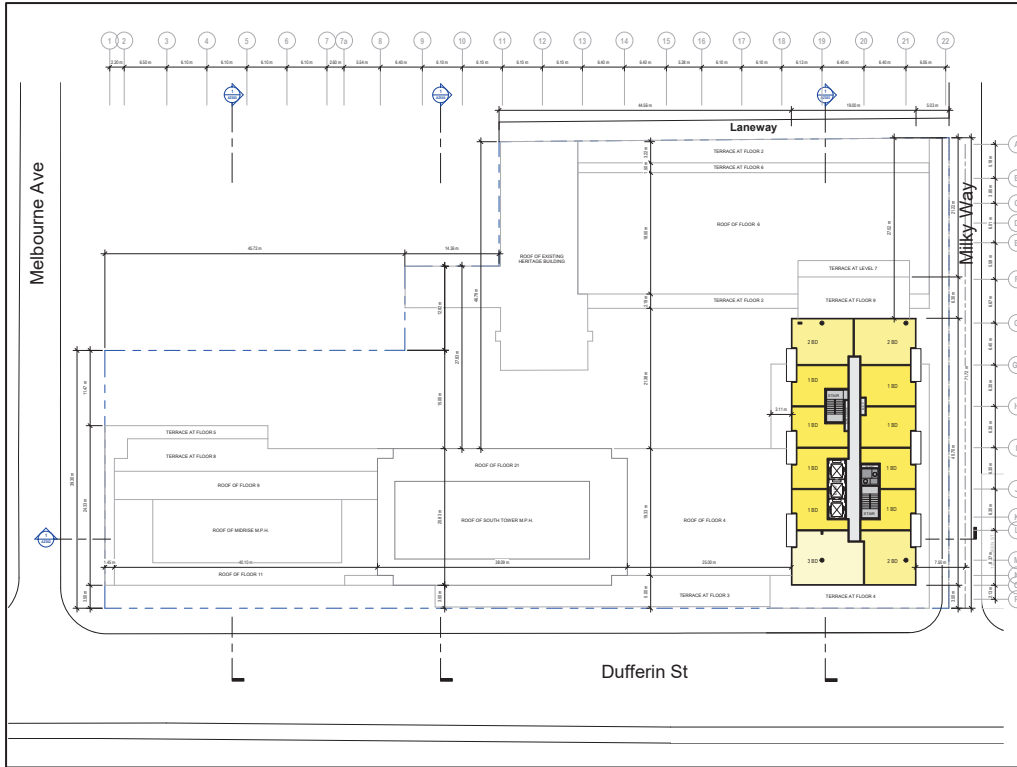


Figure 29 23rd – 25th Floor Plan (Sweeny & Co)

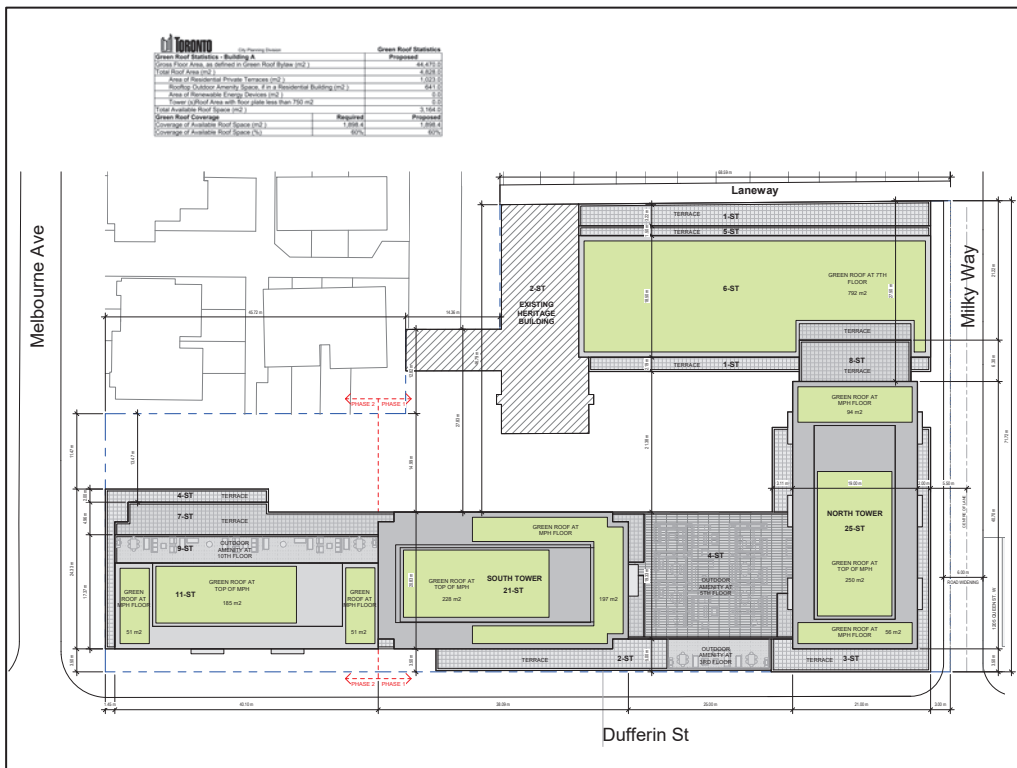


Figure 30 Roof Plan with Green Roof Areas (Sweeny & Co)

7.5. Architectural Design Approach

The buildings are clad with a combination of brick, metal panels and stone. Four dominant brick colours being buff, red, light brown and dark grey can be found in Parkdale and have been incorporated to give a better sense of context and familiarity to the project. Glazing and spandrel panels are also part of the exterior systems and provide a balanced façade of solid vs window openings

The typical floors of the two towers, while being different from each other, are characterized by the presence of an extruded grid system that creates a three-story bay subdivision. A second imposed and repeated pattern completes then the towers' composition.

The architectural form at the base is differentiated by the presence of volumes that are broken down to a smaller scale along Dufferin Street and the courtyard. The brick materiality and the retained facades add a sense of belonging at the pedestrian level.

The mechanical penthouse levels at the tops of the towers are clad in a combination of masonry and precast concrete and are intended to be read as an extension of the tower top.

7.6. Landscape Design Approach

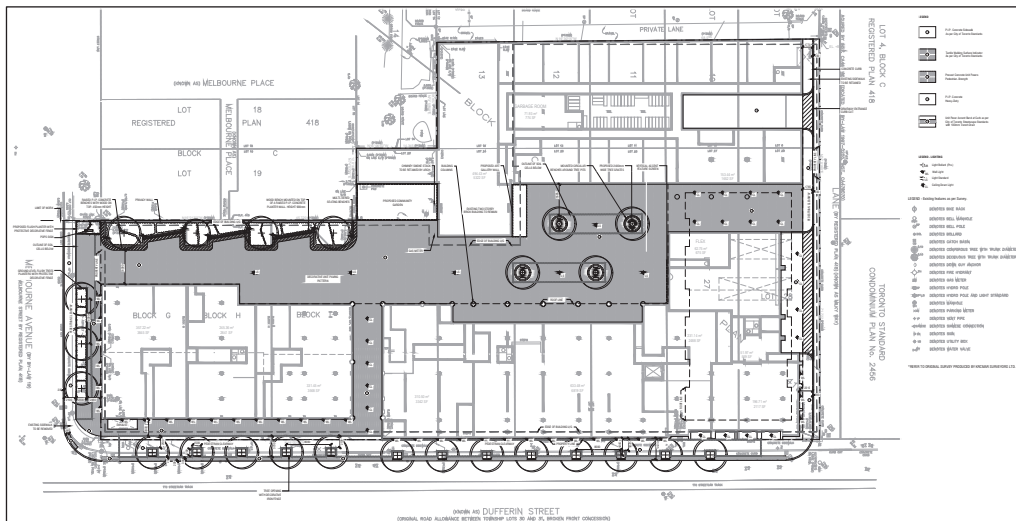


Figure 31 Landscape Plan (Land Art Design)

The application includes the proposal of the widening of the public boulevard along the Dufferin Street frontage allows for a minimum of a 2.1m pedestrian clearway towards the north end and 2.2m pedestrian clearway along the south end of the street, where the building setback provides an additional 3m of frontage for circulation and retail presence. The widened boulevard also allows for planting of 13 large native shade trees along Dufferin.

The central courtyard has access from Dufferin St., Melbourne Ave., and Milky Way, the side street to the north.

The courtyard is designed as an open space where flexible programming and retail spill-over can occur and create a community market feeling. There are four large-growing shade trees with built-in benches for a shaded seating area. Pedestrian level lighting also creates a feeling of intimate spaces in the evening hours.

The POPS serves as connection to the courtyard and features a privacy wall between it and the adjacent properties to the west, a lushly planted raised open-planting bed with 4 large-growing shade trees. The raised planting bed is retained by a two-tiered seat-wall with wood top benches which allow for seating on two levels. The seating is articulated to create nooks for more intimate seating arrangements.

The rooftop amenity terraces offer a central seating area with sofas and fireplaces, intimate booths for dining or lounging and features views of the courtyard to the west and Dufferin Street to the East. The terrace is buffered from adjacent units and from prevailing wind flow using decorative metal screens and planting.

8. Urban Design Analysis

The subject property is in the South Parkdale area, adjacent to the west part of the Garrison Common North area and the larger West Queen West area. This part of Toronto, and in particular, areas to the north and north east along Queen and Dufferin Streets, is one where numerous rezonings and/or Official Plan amendments permitting tall and mid-rise buildings have been approved by City Council and the LPAT/OLT.

The Official Plan and the City’s Tall Building Guidelines provide the basis for an urban design approach to the understanding of the existing and planned built form and context of the area.

In the case of the policies related to urban design and the development criteria of the Official Plan, this analysis focuses primarily on questions of compatibility, transition and the mitigation of impacts. Unlike some nearby areas, there is no secondary plan for the area in which the subject property is located.

The City’s Tall Building Guidelines focus on more detailed recommendations regarding building design.

In 2017 the Toronto Preservation Board endorsed the West Queen West HCD Study and the preparation of two HCD Plans for West Queen West and Parkdale Main Street. They also endorsed the identification and preliminary evaluation of potential HCDs in or around the Parkdale Residential area. The Study is in progress at the time of the writing of this report. The properties facing Queen Street West, north of Milky Way are included in the Parkdale Main Street study area but are not identified as contributing buildings.

8.1. Building Height and Massing

The proposed maximum tower height of 25 storeys (86.35 metres to the top of the mechanical penthouse) is appropriate and will fit harmoniously with the existing and planned built form context in the vicinity which includes a number of built and or approved tall buildings.



Figure 32 Context View (Sweeny &Co)



Figure 33 View from Dufferin Looking South West (Sweeny &Co)

As an addition to the immediate vicinity, and judged on its own merits, the proposed development will be seen as a part of an established cluster of mid-rise and tall buildings in the area.

Because of their slender proportions and relatively small floor plates above the tall base buildings (approximately 750 square metres) shadow impacts will be minimal and any net new shadows that result will move quickly throughout the day. These same characteristics will minimize the presence of the towers in relation to views of the sky from the nearby public realm.

The mid-rise building elements of the proposed development are set back from the property lines of adjacent *Neighbourhoods* designated lands to the west and include appropriate stepbacks above their respective bases in response to the presence of those lands.

The position of the towers relative to Dufferin Street are appropriate. They respect the character of the retained industrial building elements and support the maintenance of a mid-block courtyard on the subject property.

The North Tower is located approximately 46.3 metres south of the south side of the Queen Street West right of way. At that distance, it will have minimal visual presence from the public realm. The incremental shadow impacts from the proposed development on the nearby public realm and properties are acceptable, falling primarily on the rail corridor and areas currently occupied and/or shadowed by existing buildings. The incremental shadow impacts on Queen Street West and Dufferin Street are adequately limited.

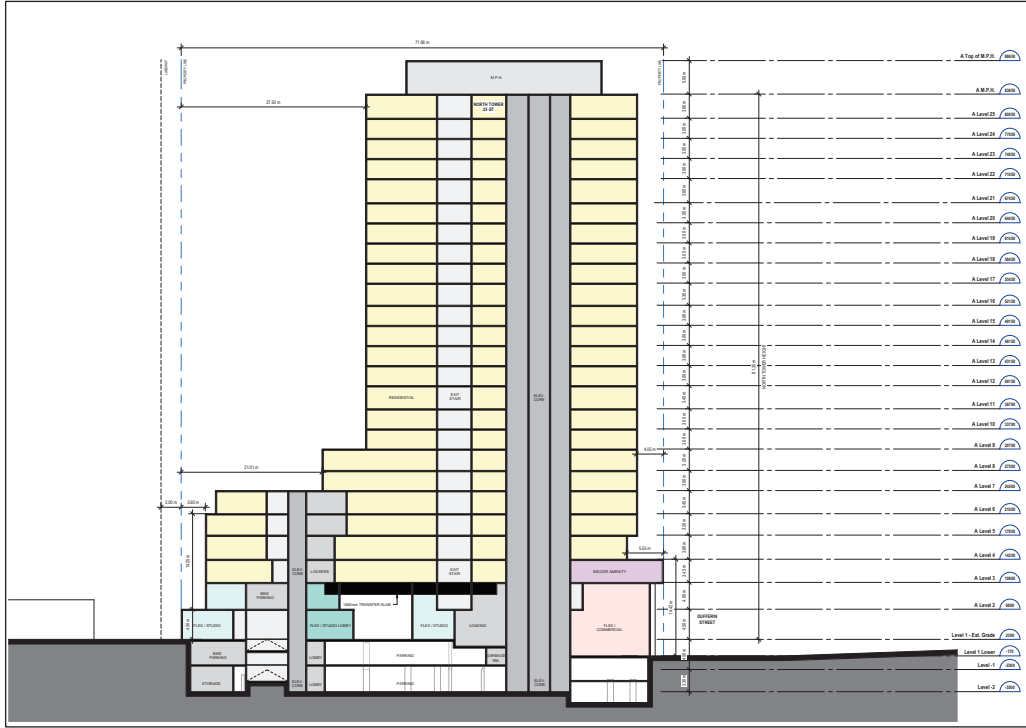


Figure 34 Cross Section Through North Tower (Sweeny & Co)

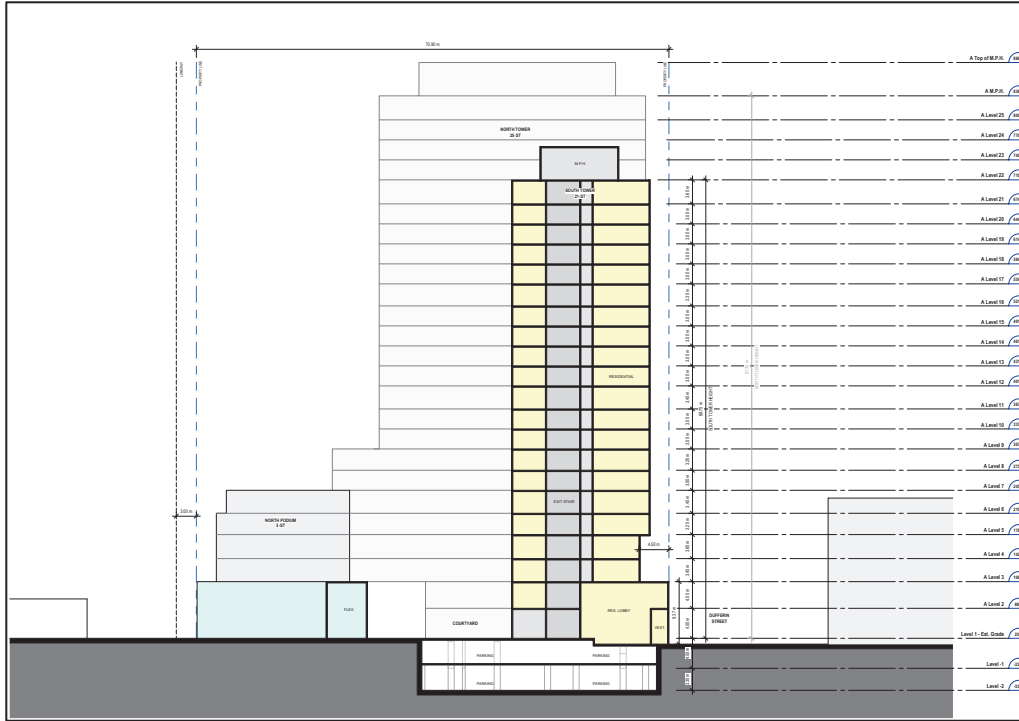


Figure 35 Section Through South Tower (Sweeny & Co)



Figure 36 Cross Section Through South Base Building (Sweeny & Co)

8.2. Shadows and Sky View

I have reviewed the Sun/Shadow Study prepared for the proposal by *Sweeny & Co.* This study focuses on the extent of shadows created by the proposal on the spring and fall equinoxes (21 March/ 21 September). The study identifies incremental increases in shadow (in relation to existing conditions and approved projects) created by the proposal.

The Shadow Study drawings were prepared using a digital model to cast and differentially render the shadows created by the existing context (warm grey shadows) approved proposals (cool grey / teal shadows), and the proposed development (orange shadows). Shadows that would be cast by the as-of-right zoning envelope are rendered in red.

The Shadow Study has been prepared to meet the City's terms of reference for such studies available on the City's website at the time of the writing of this report. Although the sun altitude and exact timing of solar noon are slightly different on March 21st and September 21st, the impacts are sufficiently similar as to be considered equal for the purposes of this report. The full shadow analysis is included in the development application package.

Spring/Fall

In the spring/fall equinoxes the shadows cast by the proposed development fall primarily on areas of the public realm and private open spaces currently shadowed by existing and approved development throughout the entire day. The sidewalk on the north side of Queen Street West, west of Dufferin Street, will experience moving patches of incremental shadow at 9:18AM and 1:18PM. The rear yards of the properties at 67-87 Gwynne Avenue will experience new shadows that are present at 9:18AM and fully gone sometime just after 10:18AM. The amphitheatre/seating area on the north side of Queen Street West that is part of the Dufferin Underpass will experience some shadow along its southern edge starting on the west at 10:18. The shadows on the amphitheatre/seating area are fully gone sometime just after 12:18PM.

After 1:18PM the shadows cast by the tall building elements fall on the rail corridor and areas shadowed by existing buildings.

Between 2:18PM and 5:18PM shadows cast by the mid-rise building elements fall on the forecourt of the residential building on the east side of Dufferin Street (#345).

In my opinion the incremental increase in shadow that will result from the proposed development is minor in nature. Where new shadows occur, they move quickly off those areas. The new shadows cast by the proposed development are therefore acceptable and conform to the relevant Official Plan policies, which are discussed in the section 6 which follows.

9. Urban Design Policy and Regulatory Context

9.1. City of Toronto Official Plan

Municipal Official Plans implement the Provincial Policy Statement and the Growth Plan. All planning decisions must be consistent with the PPS and conform to, or not conflict with, the Growth Plan. In my opinion, the site-specific rezoning required to permit this proposed mixed-use redevelopment conform to the Toronto Official Plan, April 2021 Office Consolidation.

The City of Toronto Official Plan guides land use planning for development. Among the many things that it does, the Plan directs growth and change to specific parts of the city; protects neighbourhoods and green spaces from development pressures; and integrates land use and transportation policy. Generally, there are no height or density figures in the Official Plan.

The Official Plan (April 2021) identifies the subject property and the majority of the surrounding lands to the south and west as *Neighbourhood Areas*. Properties facing Queen Street West, to the north are designated *Mixed Use Areas*. Lands to the east on Dufferin Street are designated *Regeneration Areas*.

The Planning Report prepared by Bousfields deals with the conformity of the proposed development in terms of the general policies of the City of Toronto’s Official Plan. I have worked with Bousfields on their assessment of the proposed development and I concur with their report’s conclusions.

I will discuss Official Plan policies, from an Urban Design perspective, in detail in the following sections.

9.2. Building a Successful City (OP Chapter 3)

Chapter 3 of the Official Plan sets out policies against which all applications for development will be considered and evaluated, in order to ensure that land use and development decisions and choices improve the quality of life of the City. The objectives of these policies include: making Toronto a more beautiful place; creating healthy and vibrant communities; and fostering prosperity.

This chapter acknowledges that City-building involves balancing social, economic and environmental needs and priorities.

Chapter 3 includes a number of policy directions that are relevant to the proposed development.

Built Form

With regard to Built Form (3.1) the Plan includes policies related to the public realm, general built form, and heritage resources.

The Plan anticipates the next generation of buildings of the City will arise from intensification of the existing built form stating that, *“Future development will be built on infill and redevelopment sites and will need to fit in, respecting and improving the character of the surrounding area.”* (3.1.2) In addition to responding to the site and program requirements, developments also need to address the context of the neighbourhood and City. The development should achieve transition, limit shadows on *Neighbourhoods* and *Parks*, acknowledge prominent corner sites, and coordinate parking and service areas.

With respect to built form, the Official Plan states that:

3.1.2.5 “Development will be located and massed to fit within the existing and planned context, define and frame the edges of the public realm with good street proportion, fit with the character, and ensure access to direct sunlight and daylight on the public realm by:

- a) providing street wall heights and setbacks that fit harmoniously with the existing and/or planned context;*
and
- b) stepping back building mass and reducing building footprints above the streetwall height.”*

The street wall heights on Dufferin Street and Melbourne Avenue are set by two different strategies.

On the north along Dufferin Street is defined by the 3-storey base building below the two towers, part of which includes the retention of the existing masonry façade of 360 Dufferin, augmented by a new northern element at the same scale but rendered in different materials and with different fenestration pattern.

On the south, on Dufferin Street and Melbourne Avenue the proposed 11-storey second phase mid-rise component is set back approximately 3.5 metres from the street line on Dufferin Street to create a wide sidewalk. This set back street face continues the expression of the 3-storey streetwall established to the north for approximately one half of its length before stepping up to create a 5-storey element that wraps around the corner onto Melbourne Avenue where the upper floor levels are stepped back by 1.45 metres.

The proposed development includes a recessed deeply inset “reveal” along Dufferin Street above which both the North and South Towers project forward. This approach achieves the legibility of the streetwall heights on Dufferin Street and Melbourne Avenue that is intended by this policy.

3.1.2.6. “Development will be required to provide good transition in scale between areas of different building heights and/or intensity of use in consideration of both the existing and planned contexts of neighbouring properties and the public realm.”

3.1.2.7. “Transition in scale will be provided within the development site(s) and measured from shared and adjacent property line(s).”

The relevant areas of different building heights and/or intensity of use are: the *Neighbourhoods* designated area to the west and south consisting of house form buildings on properties of a varied size that face onto Gwynne and Melbourne Avenues; and the properties immediately to the north facing Queen Street West consisting of the 3-storey Parkdale Community Health Centre and the 8-storey mixed use building at 1205-1209 Queen Street West.

Transition to the properties to the west is provided primarily through a combination of separation distance and stepping building forms within the subject property. On both the north and south blocks the proposed development establishes tall building separation distances to the properties to the west of over 27.0 metres, with the exception of a small area on the south west corner of the South Tower, where the tall building setback is approximately 15.0 metres to properties on Melbourne Place.

Transition to the properties to the south is provided in the form of building setbacks on the mid-rise building on the south block facing the street and the substantial tower setback of the South Tower.

Transition to the north is provided in the form of substantial tower setbacks from Queen Street West above the existing properties which themselves establish the street wall and public realm relationships with the street.

3.1.2.9. “The design of new building facades visible from the public realm will consider the scale, proportion, materiality and rhythm of the façade to:

- a) ensure fit with adjacent building facades;*
- b) contribute to a pedestrian scale by providing a high quality of design on building floors adjacent to and visible from the public realm;*
- c) break up long facades in a manner that respects and reinforces the existing and planned context; and*

d) ensure grade relationships that provide direct access and views into and from the public realm.”

The proposed street facing elements of the proposed development include the retention and re-use of the existing 2-1/2-storey masonry elevation of 360 Dufferin Street which has limited street facing openings other than punched windows. Minor alterations to this façade are proposed to activate this segment of the street wall beyond what currently exists. The south end of the retained masonry façade is also retained and wraps around the corner for two bays into the proposed entry point into the courtyard, which replicates the mid-block connection which currently exists on the property, beyond which a tall glazed element encloses the South Tower residential lobby. The northern part of the street wall on Dufferin Street is lined with a new 3-storey corner element that features a tall ground floor with multiple points of entry into the “flex-commercial” space and the North Tower residential lobby proposed in this area.

The setback masonry street faces of the mid-rise building on the south block is proposed to have multiple large openings into the points of entry into the “flex-commercial” space.

Together, the new wide openings, the setback of the mid-rise building and the proposed entry point into the courtyard, which replicates the mid-block connection which currently exists on the property, break up the long Dufferin Street façade that respects and reinforces the existing pattern of building along the street faces and support the retention and augmentation of the existing mid-block courtyard and the proposed mid block POPS space.

All of the street facing elements will have direct grade related access. Where the existing buildings had partially raised ground floors along Dufferin Street, new floor levels which correspond to the grade of the adjacent sidewalks have been proposed and a small number of existing window openings have been extended to meet grade.

3.1.2.10. “Development will promote civic life and provide amenity for pedestrians in the public realm to make areas adjacent to streets, parks and open spaces attractive, interesting, comfortable and functional by providing:

- a) improvements to adjacent boulevards and sidewalks including sustainable design elements, which prioritize street trees and may include one or more of the following: shrubs, hedges, plantings or other ground cover, permeable paving materials, bio-retention swales, street furniture including seating in various forms, curb ramps, waste and recycling containers, energy efficient lighting and bicycle parking facilities;*
- b) co-ordinated landscape improvements in setbacks to enhance local character, fit with public streetscapes, and provide attractive, safe transitions between the private and public realms;*
- c) weather protection such as canopies and awnings;*
- d) landscaped open space within the development site;*
- f) safe, direct pedestrian routes and tree plantings throughout the site and within surface parking lots, where possible; and*
- g) public art, where the developer agrees to provide this.”*

Replacement of all existing sidewalks and improvements to boulevards are proposed on both street frontages.

The existing retained masonry façade of 360 Dufferin Street is being retained in its current position, which limits improvements to the public realm across its face. The new base building of the north tower adopts a similar alignment and replicates the position of the existing building in this location which is being replaced.

The mid-rise building on the southern end of the subject property will be set back 3.5 metres from Dufferin Street, in order to provide a wider sidewalk with a wide pedestrian clearway and new street trees where none currently exist.

Two entry points, over which proposed buildings are located provide weather protected pedestrian access to the courtyard from Dufferin Street and Milky Way.

Additional weather protection in the form of a fixed canopy over the mid-block entry point from Dufferin Street to the courtyard and large pivoting overhead doors into the “flex-commercial” space in the mid-rise buildings are proposed. Similarly configured large opening overhead doors are proposed on the 6-storey mid-rise building into the courtyard and the private laneway, where “flex-studio” spaces are proposed. In addition, a colonnade is proposed across the courtyard facing “flex-commercial” spaces below the South Tower.

Landscaped open space, in the form of the mid-block courtyard and proposed POPS which serve both residential and commercial users. is proposed at grade.

Safe, direct pedestrian routes are proposed in the two aforementioned entry points, the edges of which are both activated by residential lobby spaces and “flex-commercial” or “flex-studio” space which face directly into them. The POPS located in the southern portion of the subject property will provide an additional connection into and out of the courtyard which will be lined with “flex-commercial” space that faces directly into it on the east side. The west side of the POPS is proposed as a landscaped element whose role is in part to provide a screen along the west edge of the POPS adjacent to the existing residential buildings.

A public art program that, among other things, provides opportunities for local artists to exhibit their work is being developed for the courtyard and the retained heritage building and its greenhouse extension, which are intended to be used as a flex-community space.

3.1.2.11. “New indoor and outdoor shared amenity spaces provided as part of multi-unit residential developments should be high quality, well designed, and consider the needs of residents of all ages and abilities over time and throughout the year.”

Residential indoor and outdoor amenity spaces are proposed in multiple locations throughout the proposed development. Two large indoor amenity spaces are proposed on the second floors of: the base of the South Tower; and the mid-rise building to the south that will be constructed in the second phase.

A third large indoor amenity space and a small associated roof top outdoor amenity space are located in the base building of the North Tower.

All three of these large indoor amenity spaces look out over both the courtyard and the surrounding streets.

A large roof top outdoor amenity space that looks out over both the courtyard and Dufferin Street is located on the roof of the 4th floor, in the gap between the North Tower and the South Tower. A small associated indoor amenity space is located in the 5th floor in the North Tower.

3.1.2.12. “Non-residential development is encouraged to provide high-quality and well designed indoor and outdoor amenity space.”

The proposed “flex-studio” spaces that are proposed on the west side of the courtyard are proposed to have access to the courtyard itself and to the indoor and outdoor amenity spaces located in the complex.

3.1.2.13. *“Outdoor amenity spaces should:*

- a) be located at or above grade;*
- b) have access to daylight;*
- c) have access to direct sunlight, where possible;*
- d) provide comfortable wind, shadow and noise conditions;*
- e) be located away from and physically separated from loading and servicing areas;*
- f) have generous and well-designed landscaped areas to offer privacy and an attractive interface with the public realm;*
- g) accommodate existing and mature tree growth; and*
- h) promote use in all seasons.”*

All of the proposed outdoor amenity spaces meet the criteria of this policy.

Based on the foregoing, in my opinion, the proposed development conforms to the built form polices found in Section 3.1.2.

3.1.3 Built Form - Building Types

Mid Rise

3.1.3.4. *“Mid-rise buildings will be designed to:*

- a) have heights generally no greater than the width of the right-of- way that it fronts onto;*
- b) maintain street proportion and open views of the sky from the public realm by stepping back building massing generally at a height equivalent to 80% of the adjacent right-of-way width; and*
- c) allow for daylight and privacy on occupied ground floor units by providing appropriate facing distances, building heights, angular planes and step-backs.”*

3.1.3.5. *“Mid-rise buildings on corner sites with different right-of-way widths will have building heights along each street edge that relate to their corresponding right-of-way width.”*

3.1.3.6. *“Mid-rise buildings on deep sites should be designed to provide and frame accessible and well-proportioned open spaces that have access to sunlight and daylight.”*

Dufferin street south of Queen Street West has a very unusual street cross section. The right-of-way of Dufferin Street is approximately 20.0 metres in the segment opposite the subject property. The existing 8-storey development immediately opposite at 345 Dufferin Street is a long “C” shaped slab building the main body of which is set back more than 21 metres. The existing buildings on the west side of the street, the subject property and the 8-storey mixed use building at 1205-1209 Queen Street West have minimal or no setbacks from the street line.

While a typical mid-rise building design solution would be to step back building the massing generally at a height equivalent to 80% of the adjacent right-of-way width, in this case, in my opinion, the atypical street

condition can sustain a taller building without the conventional setbacks and still achieve appropriate access to sunlight and sky view.

The proposed 11-storey building occupies the corner of Dufferin Street and Melbourne Avenue. The building includes a setback above the 4th floor facing Melbourne Avenue.

The west facing walls in the north west 6-storey mid-rise building that contain residential windows are set back between 3.22 and 3.82 metres from the private laneway and between approximately 6.22 and 6.82 metres from the rear lot lines of the residential properties facing Gwynne Avenue west of the private laneway.

The west facing walls in the south east 11-storey mid-rise building that contain residential windows are set back a minimum of 11.47 metres from the side lot lines of the residential properties facing Melbourne Avenue and the rear lot lines of the residential properties facing Melbourne Place.

With such configurations the proposed mid-rise buildings satisfy the intent of these policies.

Tall Buildings

3.1.3.7. *“Tall buildings are generally greater in height than the width of the adjacent right-of-way.”*

3.1.3.8. *“Tall buildings should typically be designed to consist of three parts – a base, a tower and a top – carefully integrated into a single whole.”*

The proposed tall buildings which at 21 and 25 storeys are greater in height than the width of the adjacent right-of-way have tripartite compositions with an expressed base, tower and top

3.1.3.9. *“The base portion of tall buildings should:*

- a) respect and reinforce good street proportion and pedestrian scale; and*
- b) be lined with active, grade-related uses.”*

As discussed in detail above, the base buildings elements have been scaled to extend the scale of the retained existing facade on Dufferin Street. In this way the existing street wall of this section of Dufferin Street is retained in the proposed development. The Dufferin Street frontages of the base building elements contain active uses in the form of the proposed “flex-commercial” spaces.

3.1.3.10. *“The tower portion of a tall building should be designed to:*

- a) reduce the physical and visual impacts of the tower on to the public realm;*
- b) limit shadow impacts on the public realm and surrounding properties;*
- c) maximize access to sunlight and open views of the sky from the public realm;*
- d) limit and mitigate pedestrian level wind impacts; and*
- e) provide access to daylight and protect privacy in interior spaces within the tower.”*

The incremental new shadows created by the proposal are minimized by mix of mid-rise and tall buildings and the point tower form of the tall buildings. They move quickly during the day and fall primarily on the nearby rail corridor and areas currently occupied and/or shadowed by existing buildings. The cumulative effect of the additional shadow cast by the proposed development is acceptable and adequately limits shadow impacts on

neighbouring properties and open spaces – including the City streets. There are no City parks in the immediate vicinity.

In my opinion, the small amount of shadow from the proposed development, discussed in Section 5 above, is adequately limited and minimized on the public realm.

Sky views are appropriately considered by the proposed development. The approach taken to the height and massing of the tall building ensures adequate sky views from nearby streets and open spaces. In this respect the proposal conforms to the specific Official Plan Policies found in these sections regarding framing of streets and ensuring adequate sky views.

Light, view and privacy impacts are generally addressed through a combination of spatial separation, orientation and mitigating measures between buildings. The underlying I1-D2 (in By-law 438-86) do not provide relevant light view and privacy standards to apply to the proposed development in relationship to nearby residential or mixed-use buildings. The CR standards of By-law 438-86 or the CR (SS2) zoning standards of By-law 569-2013, all of which are more relevant to the form of development that is proposed here generally require setbacks for walls containing windows as follows:

438-86 8(3)II(1.) “Window separation

(a) No person shall erect or use a residential building or a mixed-use building on a lot in a CR or Q district in which a window of a dwelling unit (other than a window of a kitchen or bathroom) or a window of a dwelling room in the building is closer than:

(i) 11 metres to a window of another dwelling unit (other than a window of a kitchen or bathroom) or a window of a dwelling room on the same lot; or

(ii) 5.5 metres to a wall, or to a lot line that is not a street line or that does not abut a public park or a UOS district. (1997-0275)”

569-2013 40.10.40.70 “Setbacks

(2) Development Standard Set 2 - Building Setbacks

*In the CR zone subject to Development Standard Set 2 (SS2), a **building** or **structure** is subject to the following:*

*(B) the **building** must be set back:*

*(i) at least 7.5 metres from the **rear lot line**; or*

*(ii) where the **rear lot line** abuts a **lane**, at least 7.5 metres from the **lot line** of the **lot** abutting the **lane** on the opposite side of the **lane**; and*

*(C) where the **main wall** of a **building** has windows or openings, the **main wall** must be set back at least 5.5 metres from a **side lot line** that is not adjacent to a **street** or **lane**, otherwise no **building setback** is required;*

*(E) if a lot abuts a **lot** in the O, ON or OR zone or the Residential Zone category or Residential Apartment Zone category, or if a **lot** is separated from a **lot** in the O, ON or OR zone or the Residential Zone category or*

*Residential Apartment Zone category by a lane, no **building** or **structure** on the **lot** in the CR zone may penetrate a 45 degree **angular plane** projected:*

- (i) over a shallow **lot**, along the entire required **rear yard setback**, starting at a height of 10.5 metres above the average elevation of the ground along the **rear lot line**; and*
- (ii) over a deep lot, along the entire required **rear yard setback**, starting at a height of 7.5 metres above the average elevation of the ground along the **rear lot line**; and [By-law: 607-2015]*

The distances between the vast majority of the residential windows in the proposed development and the existing buildings in the vicinity satisfy the separation distances found in By-laws 569-2013 and intent of those recommended in the Tall Building Guidelines which are discussed below in Section 9.3.

1205-1209 Queen Street West, an 8-storey mixed use building on the north side of Milky Way has residential windows on its south face that are located approximately on the north edge of Milky Way. These walls containing these windows were approved without setbacks such as those required in the applicable underlying zoning standards for such walls.

1229 Queen Street West is a 3-storey building, also on the north side of Milky Way facing the subject property, which has no residential uses.

The north face of the base building of the North Tower, which contains residential windows on the 3rd floor only, is set back 5.5 metres from the centre line of Milky Way and separated from south facing main wall of 1205-1209 Queen Street West by a minimum of approximately 8.5 metres.

The north face of the North Tower, which contains residential windows is set back 7.5 metres from the centre line of Milky Way and separated from south facing main wall of 1205-1209 Queen Street West by approximately 10.5 metres. This condition exists on the 4th through 8th floors. Above the 8th floor the North Tower is above the uppermost floor of 1205-1209 Queen Street West.

In both cases the windows in the proposed development have been located so that they are not directly opposite and do not overlap with those on the existing south face of 1205-1209 Queen Street West. This building is relatively recently constructed and therefore does not represent a site for a future tall building which would necessitate a larger tower setback on the proposed development.

The west face of the South Tower is located well over 27.0 metres from the rear lot lines of the deepest of the residential properties facing Gwynne Avenue. A small portion of the south west corner of the South Tower is located 14.98 metres from the rear lot line of the residential properties facing Melbourne Place.

In my opinion it is appropriate to consider the actual distance between tall buildings (including their base buildings) at the scale of the urban block, understanding that individual tower setbacks from property lines internal any particular urban block may vary from those recommended in the Tall Building Guidelines.

Based on the foregoing, in my opinion adequate and appropriate standards of light view and privacy will be achieved for both existing and future developments.

Pedestrian level wind impacts have been considered in the proposed design. Gradient Wind Engineers have prepared a report which concludes that:

“...the future wind conditions over most grade-level pedestrian wind sensitive areas within and surrounding the study site will be acceptable for the intended uses on a seasonal basis. Exceptions include the covered walkway separating Phases 1 and 2, as well as several internally facing commercial and lobby entrances, for which mitigation is recommended.

The Level 3 amenity terrace will experience wind conditions comfortable for sitting or more sedentary activities during the summer months without the need for mitigation. The Level 5 and Level 11 amenity terraces variably exceed the sitting criterion and appropriate mitigatory measures are recommended...

Within the context of typical weather patterns, which exclude anomalous localized storm events such as tornadoes and downbursts, no areas over the study site were found to experience conditions that could be considered unsafe.”

3.1.3.11. *“Policies 3.1.3.10 a) through 3.1.3.10 e) should be achieved by:*

- a) stepping back the tower from the base building;*
- b) generally aligning the tower with, and parallel to, the street;*
- c) limiting and shaping the size of tower floor plates above base buildings;*
- d) providing appropriate separation distances from side and rear lot lines as well as other towers; and*
- e) locating and shaping balconies to limit shadow impacts.”*

The proposed development includes a recessed deeply inset “reveal” along Dufferin Street above which both the North and South Towers project forward. This approach achieves the differentiation and legibility of the base and tower aspects of the proposed tall buildings that is intended by this policy.

Both towers are aligned with Dufferin Street, are rectangular in form and have floor plates of 750 square metres. A separation distance between the two towers of 25.0 metres is proposed.

The majority of the North Tower is above the height of the 8-storey building at 1205-1209 Queen Street West where no future tall building will occur and establishes tower setbacks of approximately: 27.5 metres above the 8th floor to the west edge of the private laneway to the west; and 32.5 metres to the rear lot lines of the *Neighbourhoods* designated properties facing Gwynne Avenue to the west.

South Tower setbacks to the properties facing Gwynne Avenue to the west range between approximately 27.8 and 51.7metres.

A small portion of the south west corner of the South Tower is located 14.98 metres from the rear lot lines of the *Neighbourhoods* designated properties facing Melbourne Place to the west (#1, #3, #5, #7).

Recessed or partially recessed balconies are proposed on both towers.

3.1.3.12. *“The top portion of a tall building should be designed to:*

- a) integrate roof top mechanical systems into the building design;*
- b) contribute to the surrounding skyline identity and character; and*
- c) avoid up-lighting and excessive lighting.”*

The mechanical penthouses of both proposed towers are set back substantially from all faces of the towers in order to have a minimal visual presence when viewed from the nearby public realm. The design approach taken to the towers, which are differentiated in terms of materials and facade treatments, is to create a simple rectangular form where there is no separately expressed top.

The two towers will generate dynamic new skyline elements which will contribute to an emerging rhythm of tall buildings that punctuates key points along southern Dufferin Street - including new development at the King-Dufferin intersection and recently constructed tall buildings north of Queen Street West, beyond the rail corridor.

3.1.5 Development on Properties on the Heritage Register

3.1.5.26. “New construction on, or adjacent to, a property on the Heritage Register will be designed to conserve the cultural heritage values, attributes and character of that property and to mitigate visual and physical impact on it.”

The proposed development is adjacent to the Parkdale Main Street Heritage Conservation Area, study area. This HCD is still in process. The buildings immediately adjacent to the north, within the study area are relatively recently built, not listed or designated and are not considered to be “contributing” buildings within the study which was adopted by the Toronto Preservation Board on 14 June, 2022.

The proposed development is also adjacent to four properties that are listed on the City’s Heritage Register – 1-7 Melbourne Place.

The proposed development includes significant setbacks and a stepping building form facing these listed properties, as well as a proposed landscape screen along the shared property line. In my opinion these measures are sufficient to satisfy the concerns regarding heritage conservation and mitigation of visual and physical impacts on them.

In summary, in my opinion the proposed development conforms to the urban design and built form policies of the Official Plan.

9.3. City Wide Tall Building Guidelines

The City of Toronto city-wide Tall Building Design Guidelines were updated and consolidated in 2013. These guidelines now also include a Supplemental Vision and Design Guidelines for Tall Buildings in Downtown Toronto. The proposed building is considered a "tall building" and the city-wide Tall Building Design Guidelines apply and should be considered and evaluated. The Supplemental Vision and Design Guidelines for Tall Buildings in Downtown Toronto do not apply, as the subject property is outside of *Downtown*.

The Tall Building Guidelines contain recommendations regarding the following:

- 1.1 Context Analysis
- 1.3 Fit and Transition in Scale
- 1.4 Sunlight and Sky View
- 1.6 Heritage Properties and Heritage Conservation Districts

- 2.1 Building Placement
- 2.2 Building Address and Entrances
- 2.3 Site Servicing, Access, and Parking
- 3.1 Base Building
 - 3.1.1 Base Building Height and Scale
 - 3.1.2 Street Animation
 - 3.1.3 First Floor Height
 - 3.1.4 Façade Articulation and Transparency
- 3.2 Middle
 - 3.2.1 Floor Plate Size and Shape
 - 3.2.2 Tower Placement
 - 3.2.3 Separation Distances
 - 3.2.4 Tower Orientation and Articulation
- 3.3 Tower Top
- 4.1 Streetscape and Landscape Design
- 4.2 Sidewalk Zone
- 4.3 Pedestrian Level Wind Effects
- 4.4 Pedestrian Weather Protection

For tower elements, the Tall Building Design Guidelines recommend a tower setback of 12.5 metres from side and rear property lines, or in the case where a public laneway is present to the centre line of the laneway, measured to the external walls of the building (i.e. balconies are permitted within the setback zone).

The proposed development satisfies the intent and meets the specifics of the majority of the recommendations found in the Tall Building Guidelines related to Fit and Transition in Scale, Sunlight and Sky View, Heritage Properties and Heritage Conservation Districts, Building Placement, Building Address and Entrances, Site Servicing, Access, and Parking, Base Building Height and Scale, Street Animation, First Floor Height, Façade Articulation and Transparency, Tower Orientation and Articulation, Tower Top, Streetscape and Landscape Design, Sidewalk Zone, Pedestrian Level Wind Effects and Pedestrian Weather Protection.

While the design approach in the proposed development differs from some recommendations of the Tall Building Guidelines, in my opinion, appropriate setbacks and transitions are provided. The proposed development generally satisfies the intent of the Tall Building Guidelines and responds to site conditions as discussed below.

Tower setbacks to nearby existing buildings have been discussed in section 6.1.1 above. The proposed tower setbacks of both the North Tower and South Tower generally satisfy the intent of the Tall Building Guidelines.

The proposed 7.5 metre tower setback of the North Tower from the north property line – opposite which is an existing building that is much lower than the proposed tower and unlikely to redevelop as a tower in the future, in my opinion is appropriate and will not result in any undue adverse impacts on residents of that building or the public realm of Queen Street West, to the north.

In my opinion, appropriate light view and privacy conditions between principal windows can be achieved at the proposed North Tower setbacks of both base and tower. The north face of the proposed tower facing the existing building to the north occurs primarily above the roof of that building and therefore will not result in an appreciable difference in the view of the sky available from the public realm that surrounds the urban block.

The proposed tall buildings exhibit the tripartite composition than that which is recommended by the Tall Building Guidelines. The proposed development creates base buildings with street wall heights that are similar to the existing buildings which currently frame Dufferin Street – and include a portion of those exiting facades. However, in certain areas the tower stepbacks above their respective bases buildings are less than the 3.0 metres recommended in the Guidelines but are, in my opinion, sufficient to allow the streetwalls to be legible and, as confirmed in the Wind Study, result in no undue adverse wind impacts on the public realm or adjacent properties.

The towers are proposed as relatively simple rectangular prisms, with articulation provided by varied approaches provision of framing elements that diminish the visual presence of balconies, different materials and colours between the two towers and the substantial stepbacks of the Mechanical Penthouses above the tops of the towers.

The typical tower floor plate of 750 square satisfies the maximum of 750 square metres recommended in the Guidelines.

As described elsewhere in this report in detail, the proposed tower elements are highly articulated and, in my opinion, satisfy the recommendations on floor plate size and articulation.

On balance, in my opinion, the proposed development responds well to the intent and objectives of the recommendations of the City-wide Tall Building Design Guidelines and the relevant Official Plan tall building policies as discussed in section 6.1.1 above.

9.4. City of Toronto Zoning By-law

The City's harmonized Zoning By-law 569-2013 has been passed by City Council but appealed to the Local Planning Appeal Tribunal (LPAT) (now OLT). The subject property is not included in the City's new Zoning By-law.

By-law 438-86, which is the in-force zoning by-law, zones the subject property "I1-D2".

The proposed development requires an amendment to the Zoning By-law for an increase in height and other development standards that are intended to be tailored on a site-specific basis such as setbacks.

Draft Zoning By-law Amendments to City of Toronto Zoning By-law 438-86 and 569-2013 have been prepared by Bousfields.

I have reviewed the Zoning By-law Amendments and concur with Bousfields that, if approved, these would be appropriate instruments to permit the proposed development.

9.5. Site Plan Control

The proposed development is subject to Site Plan Control. An application for Site Plan Approval will follow at a later date.

10. Conclusion

The proposed development fits appropriately into its existing and planned contexts.

It achieves important urban design objectives found in the relevant policy and guideline documents. Chief among these are:

- Framing the street edge at an appropriate scale;
- Providing appropriate levels of light view and privacy to existing adjacent developments; and
- Providing adequate access to light and sky views from the public realm and appropriately limiting shadowing and adverse pedestrian level winds on neighbouring streets, properties and open spaces.

The proposed development is a high-quality architectural design that responds appropriately to the subject property and its surroundings.

The proposed development constitutes a good urban design response to its surroundings, which achieves the goals of the City-Wide Tall Building Guidelines.

Key urban design considerations in the design of the proposed development related to the Tall Building Guidelines include:

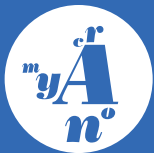
- Provision of appropriately scaled street defining buildings;
- Appropriate step backs of the proposed tall buildings above the street defining base buildings which themselves respect the scale of the existing buildings along both Dufferin Street and Melbourne Avenue;
- Access to the residential lobbies directly from Dufferin Street and Milky Way as well as the proposed courtyard and POPS;
- Creation of slender tower elements that establish appropriate separation distances to existing, buildings in the immediate vicinity, including those in the adjacent *Neighbourhoods* designated lands, which will not result in undue adverse impacts in terms of shadow, wind or sky views;
- Provision of loading facilities that are appropriately located away from view from the public realm;
- Provision of much needed improvements to the public realm including activation of the publicly accessible courtyard and new street trees on the Dufferin Street frontage and along Melbourne Avenue; and
- Providing adequate indoor and outdoor amenity areas to future residents as well as improved streetscaping along the street frontages.

The proposed development exhibits an appropriate architectural and urban design response to its context, is good planning and is in the public interest.

Acronym Urban Design and Planning
Mark Sterling Consulting Inc.

A handwritten signature in black ink, appearing to read "Mark Sterling". The signature is written in a cursive, flowing style.

Mark Sterling BES, B.Arch, OAA, FRAIC, MCIP, RPP
Principal



Acronym

Urban Design & Planning